Massachusetts R h od e W Y O I sland Sound PROJECT LOCATION N.T.S.

ORIDGED OF COMPORATION OF THE POPULATION OF THE

OWNER:

CITY OF BRIDGEPORT DEPARTMENT OF PUBLIC PURCHASES 999 BROAD STREET 2ND FLOOR BRIDGEPORT, CONNECTICUT 06604 P 203.576.7161

NOTES:

ALL DIMENSIONS ARE IN FEET (FT) UNLESS NOTED OTHERWISE

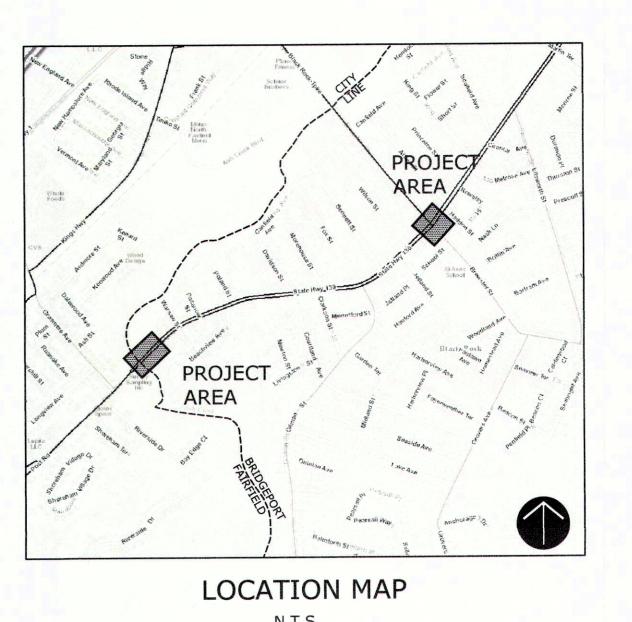
THE CITY OF BRIDGEPORT STANDARD TECHNICAL SPECIFICATIONS AND STANDARDS FOR STREETS AND ROADS, TRAFFIC, AND STREETSCAPE CONSTRUCTION INCLUDING ALL SUPPLEMENTS THERETO GOVERN. THE 2016 SPECIFICATIONS FORM 817 INCLUDING SUPPLEMENTAL SPECIFICATIONS GOVERN WHERE REFERENCED BY THE CITY OF BRIDGEPORT STANDARD SPECIFICATIONS AND SUPPLEMENTS THERETO.

CONSTRUCTION DOCUMENTS

BLACK ROCK BUSINESS DISTRICT STREETSCAPE IMPROVEMENTS

IN THE

CITY OF BRIDGEPORT, CONNECTICUT Joseph P. Ganim, Mayor



DECD PROJECT NO. 2014 4018 004

AUGUST 6, 2018

LIST OF DRAWINGS				
SHEET NAME	TITLE			
	TITLE SHEET			
GN	GENERAL NOTES			
EX-1	EXISTING CONDITIONS SURVEY			
EX-2	EXISTING CONDITIONS SURVEY			
IN	INDEX PLAN			
SP-02	SITE PREPARATION / DEMOLITION PLAN			
PLN-01 TO PLN-02	CONSTRUCTION PLANS			
TIE-01 TO TIE-02	CURB TIE PLANS			
L-01 TO L-02	PLANTING PLANS			
MDS-01 TO MDS-04	MISCELLANEOUS DETAILS			
	TRAFFIC CONTROL SIGNAL PLAN			

SHEET NAME	TITLE
	SIDEWALK RAMPS SHEET 1
	SIDEWALK RAMPS SHEET 2
	SIDEWALK RAMPS SHEET 3
	SIDEWALK RAMPS SHEET 4
TR-1001_01	TRENCHING & BACKFILL, ELECTRICAL CONDUIT
TR-1002_01	TRAFFIC CONTROL FOUNDATIONS
TR-1010_01	CONCRETE HANDHOLE
TR-1102_01	PEDESTALS, PEDESTRIAN SIGNALS
TR-1105_01	TRAFFIC SIGNALS & CABLE ASSIGNMENTS
TR-1107_01	PEDESTRIAN PUSH BUTTONS
TR-1108_01	CONTROLLERS
TR-1111_01	LOOP VEHICLE DETECTOR AND SAWCUT
TR-1210_04	PAVEMENT MARKING LINES AND SYMBOLS
TR-1210_09	PAVEMENT MARKINGS FOR BICYCLE LANES, PARKING STALLS AND
	RAILROAD GRADE CROSSINGS
TR-1220_01	SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS
TR-1220_02	CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES
TR-GS_01	SIGN FACE SHEET ALUMINUM R-SERIES SIGNS TYPICAL DETAILS
TR-GS_02	SIGN FACE SHEET ALUMINUM S&W SERIES SIGNS TYPICAL DETAILS
TR-GS_03	SIGN FACE SHEET ALUMINUM D, RS, E, I, & M SERIES SIGNS TYP. DETAILS
HW-507_01	TYPE "C", "C-L" & DROP INLET CATCH BASIN
HW-507_04	TYPE "C", "C-L" & ROUND PRECAST CONCRETE CB
HW-507_07	TYPE "C", "C-L" CATCH BASIN TOPS AND CURBS
HW-507_08	CATCH BASIN FRAMES AND GRATES
HW-507_10	MANHOLE - FRAME & COVER
HW-651_01	C.C.M. PIPE INSTALLATIONS IN FILL & ROCK SLOPES & PIPE TRENCH DETAIL
HW-921 01	DRIVEWAY RAMPS AND SIDEWALKS

FREEMAN OMPANIES

LANDSCAPE ARCHITECT / CIVIL ENGINEER:

FREEMAN COMPANIES 36 JOHN STREET HARTFORD, CONNECTICUT 06106 P 800.604.5141



whb

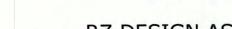
RZ Design Associates, inc

TRANSPORTATION / CIVIL ENGINEER:

VANASSE HANGEN BRUSTLIN, INC. 100 GREAT MEADOW ROAD SUITE 200 WETHERSFIELD, CONNECTICUT 06109 P 860.807.4300



THIS SEAL APPLIES TO THE FOLLOWING DRAWINGS ONLY:
GN, PLN-01, TIE-01, MDS-01,
TRAFFIC CONTROL SIGNAL PLAN



RZ DESIGN ASSOCIATES, INC. 750 OLD MAIN STREET SUITE 202 ROCKY HILL, CT 06067 P 860.436.4336

ELECTRICAL ENGINEER:



GENERAL NOTES

- 1. THE EXISTING CONDITIONS BASE PLAN WAS COMPILED FROM RECORD MAPS, RESEARCH AND OTHE'R SOURCES OF INFORMATION. THE EXISTING INFORMATION ALONG THE PROJECT ALIGNMENT WAS BASED ON FIELD SURVEY. THE UNDERGROUND UTILITIES DEPICTED HAVE BEEN PLOTTED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE CONSULTANT MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES DEPICTED COMPRISE ALL SUCH UTILITIES IN THIS AREA, EITHER IN SERVICES OR ABANDONED. THE CONSULTANT FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES DEPICTED ARE IN THE EXACT LOCATION INDICATED. THE CONSULTANT HAS NOT PHYSICALLY EXPOSED THE UNDERGROUND UTILITIES.
- 2. THE CONTRACTOR SHALL DETERMINE, PRIOR TO CONSTRUCTION, THE LOCATIONS OF ALL UTILITIES AND SHALL BE RESPONSIBLE FOR ALL DAMAGES RESULTING FROM HIS OPERATIONS. CALL BEFORE YOU DIG, 1-800-922-4456, AT LEAST 24 HOURS PRIOR TO STARTING CONSTRUCTION. THE CONTRACTOR IS TO COORDINATE ACTIVITIES WITH INDIVIDUAL COMPANY REPRESENTATIVES. THE CONTRACTOR SHALL ACCURATELY LOCATE ALL UNDERGROUND UTILITIES IN AREAS WHERE PROPOSED CONSTRUCTION MAY CONFLICT WITH THEM AND SHALL BE RESPONSIBLE FOR ALL DAMAGES.
- 3. AS CONSTRUCTION IS COMPLETED, THE CONTRACTOR SHALL REMOVE ALL EXCESS MATERIAL, DEBRIS, ETC. AND RESTORE AND/OR REPAIR ANY DAMAGE TO LANDSCAPING.
- 4. ANY PIPE LENGTHS SHOWN ARE MEASURED FROM CENTER TO CENTER OF STRUCTURES.
- 5. AREAS OUTSIDE THE WORK LIMITS DISTURBED BY CONSTRUCTION SHALL BE RETURNED TO THEIR ORIGINAL CONDITION OR BETTER AND SHALL BE GRADED TO MEET PROPOSED CONSTRUCTION AS DIRECTED BY THE ENGINEER. COST FOR THIS WORK SHALL BE THE CONTRACTORS SOLE RESPONSIBILITY AT NO COST TO THE OWNER.
- 6. THE CONTRACTOR SHALL PROCURE ALL THE NECESSARY PERMITS AND LICENSES, PAY ALL CHARGES AND FEES, AND GIVE NOTICES NECESSARY AND DUE IN CONNECTION WITH THE LAWFUL EXECUTION OF THE WORK AT NO ADDITIONAL COST TO THE CITY.
- 7. EXCAVATION OF ANY TYPE SHALL BE ACCOMPLISHED IN SUCH A MANNER THAT UNDERGROUND UTILITIES AND STRUCTURES ARE NOT DAMAGED. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ANY DAMAGE INCURRED DURING EXCAVATION OPERATIONS. ALL EXCAVATION SHALL BE IN CONFORMANCE WITH THE LATEST OSHA REQUIREMENTS.
- 8. ALL UTILITY BOXES, FRAMES, GRATES, ETC. AFFECTED BY THE CONSTRUCTION ACTIVITIES, SHALL BE RESET TO THE PROPER GRADE. ALL COST RELATED TO SUCH WORK SHALL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID.
- 9. ALL DRIVEWAYS, ROADS, STAIRS, AND SIDEWALKS DISTURBED BY THE CONSTRUCTION IN OR OUTSIDE THE PROJECT AREA SHALL BE RETURNED TO THEIR ORIGINAL CONDITION OR BETTER AND SHALL BE GRADED TO MEET PROPOSED CONSTRUCTION AS DIRECTED BY THE ENGINEER AT NO ADDITIONAL COST TO THE OWNER.
- 10. ANY UTILITY SERVICE CONNECTIONS (GAS, WATER, SEWER, ELECTRIC, ETC.) DISTURBED BY THE CONSTRUCTION ACTIVITIES SHALL BE REPAIRED AND SERVICE RESTORED. AT NO TIME SHALL ANY EXISTING BUILDING BE WITHOUT SERVICES (INCLUDING FIRE PROTECTION) WITHOUT PRIOR NOTIFICATION OF THE PARTICULAR UTILITY AUTHORITY AND PROPERTY OWNER. NO FIRE PROTECTION UTILITIES OF ANY KIND WILL BE DISRUPTED WITHOUT PERMISSION FROM THE FIRE MARSHALL. THE CONTRACTOR SHALL COORDINATE HIS WORK, PRIOR TO A UTILITY SHUTDOWN, WITH THE UTILITY COMPANY AND THE OWNER.
- 11. THE CONTRACTOR SHALL SUFFICIENTLY COVER ALL DISTURBED AREAS AT THE END OF EACH WORK DAY TO AVOID ANY RISK OF INJURY TO PEDESTRIAN OR VEHICULAR TRAFFIC. THE CONTRACTOR SHALL INSTALL TEMPORARY SUPPORT SYSTEMS OVER TRENCH EXCAVATIONS THAT ARE TAMPER RESISTANT AND SAFE FOR VEHICULAR AND PEDESTRIAN TRAFFIC. THE CONTRACTOR SHALL INSTALL BARRICADES TO PROTECT AGAINST PEDESTRIAN ACCESS. THE CONTRACTOR SHALL OBTAIN APPROVAL OF THE TEMPORARY SAFETY MEASURES BY THE ENGINEER AND THE OWNER. ALL MAINTENANCE AND PROTECTION OF BOTH PEDESTRIAN AND VEHICULAR TRAFFIC SHALL MEET THE REQUIREMENTS OF ITEM# 0971001A - MAINTENANCE AND PROTECTION OF TRAFFIC AND IS INCLUDED IN THE BID PRICE FOR THIS ITEM.
- 12. LOCATION AND ELEVATION OF ALL UNDERGROUND UTILITIES INSTALLED OR FOUND DURING CONSTRUCTION SHALL BE INDICATED BY THE CONTRACTOR ON RECORD DRAWINGS, MEASURED FROM PERMANENT SURFACE FEATURES.
- 13. WHEN EXCAVATING CLOSE TO TREES, USE ONLY HAND TOOLS. CARE SHALL BE TAKEN TO ENSURE THAT TREE ROOTS ARE NOT DAMAGED IN ANY WAY. THE INSPECTOR SHALL BE NOTIFIED IMMEDIATELY WHENEVER TREE ROOTS ARE ENCOUNTERED. FURTHER EXCAVATING AT THAT LOCATION SHALL BE AS DIRECTED BY THE INSPECTOR.
- 14. CONTRACTOR SHALL INFORM UTILITY COMPANY OF REQUIRED POLE RELOCATION AND COORDINATE HIS WORK WITH UTILITY COMPANY.
- 15. PRIOR TO OPENING AN EXCAVATION, EFFORT SHALL BE MADE TO DETERMINE WHETHER UNDERGROUND INSTALLATIONS, (I.E. SEWER, WATER, FUEL, ELECTRIC LINES, ETC.) WILL BE ENCOUNTERED AND, IF SO, WHERE SUCH UNDERGROUND INSTALLATIONS ARE LOCATED. WHEN THE EXCAVATION APPROACHES THE ESTIMATED LOCATION OF SUCH INSTALLATION, THE EXACT LOCATION SHALL BE DETERMINED BY CAREFUL PROBING OR HAND DIGGING, AND WHEN IT IS UNCOVERED, PROPER SUPPORTS SHALL BE PROVIDED FOR THE EXISTING INSTALLATION. UTILITY COMPANIES SHALL BE CONTACTED AND ADVISED PRIOR TO THE START OF ACTUAL EXCAVATION. VERIFICATION BY TEST EXCAVATION IF NECESSARY.
- 16. NO MATERIAL OR EQUIPMENT IS ALLOWED TO BE STORED OVERNIGHT WITHIN THE STREET R.O.W.
- 17. REFER TO THE SPECIAL CONDITIONS OF THIS PROJECT, SECTION 1.08-PROSECUTION AND POROGRESS FOR HOURS OF WORK RESTRICTIONS.

STANDARD ABBREVIATIONS

ABDN('D) AC AOBE ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS	IN INC INV L	INCHES INCORPORATED INVERT LENGTH OF CURVE
BC BCLC		LOC LP	LOCATION LIGHT POLE
BIT BL	BITUMINOUS BOTTOM OF LIP	LT MAX	LEFT MAXIMUM
BLDG	BUILDING	MH	MANHOLE
BM BOT		MIN MISC	MINIMUM
BOT BOW	BACK OF WALK	MON	
BSMT BW	BASEMENT BOTTOM OF WALL	MPH	
BW C	BOTTOM OF WALL CABLE	N NTS	
CATV	CABLE TELEVISION	N/A	NOT APPLICABLE
CB CC	CATCH BASIN CURVE CENTER	N/F OH	NOW OR FORMERLY OVERHEAD
CF	CUBIC FEET	Р	POWER
CIP CL, C/L	CAST IRON PIPE CENTERLINE	PC PCC	POINT OF CURVATURE POINT OF COMPOUND CURVATURE
CLF	CHAIN LINK FENCE	PCPP	PERFORATED CORRUGATED POYETHYLENE PIPE
CL&P CMP		PED PI	PEDESTRIAN POINT OF INTERSECTION
CO	COMPANY	PL, P/L	PROPERTY LINE
COMB CONC	COMBINED CONCRETE	POT PRC	
CONN, CT	CONNECTICUT	PROP	PROPOSED
CPP CSW	CORRUGATED POLYETHYLENE PIPE CONCRETE SIDEWALK	PS PT	PARKING SPACES POINT OF TANGENCY
CY	CUBIC YARD	PVC	POLYVINYL CHLORIDE
D DC	STORM MANHOLE DEGREE OF CURVATURE	R, RAD RCP	
DIA	DIAMETER	REINF	REINFORCED
DIP DOT	DUCTILE IRON PIPE DEPARTMENT OF TRANSPORTATION	RET REV	RETAINING REVISION
DWG	DRAWING	RGCDR	RADIAL GRANITE CURB DRIVEWAY RETURN
DS DWL	DESIGN SPEED DASHED WHITE LINE	RMC ROW	
DYCL	DOUBLE YELLOW CENTERLINE	RT	RIGHT
	EAST/ELECTRIC/EASTING EXISTING GRADE	S SAN	,
EL, ELEV	ELEVATION	SCH	
ELEC EMH	ELECTRIC ELECTRIC MANHOLE	SF SHLDR	SQUARE FEET SHOULDER
EOP	EDGE OF PAVEMENT	SL, S/L	STREET LINE
FES	EXISTING FLARED END SECTION	ST STA	STORM STATION
FF	FIRST FLOOR	STD	STANDARD
FT G	FEET, FOOT GAS	STRM SY	STORM SQUARE YARD
GEN GG		T TC	,
GRAN		TEL	TELEPHONE
GRC		TF TL	TOP OF FRAME TOP OF LIP
GSC	GRANITE CURVED STONE CURBING GRANITE STONE CURBING	TW	TOP OF WALL
H, HOR HC	HORIZONTAL HANDICAPPED	TYP US	TYPICAL UNITED STATES
HTFD	HARTFORD	USA	UNITED STATES OF AMERICA
HYD	HYDRANT	V, VERT W	VERTICAL WATER
		W/	WITH
		WG, WV W/O	WATER GATE/WATER VALVE WITHOUT
		XFMR	TRANSFORMER
		&: @	AND AT
		Δ	DELTA ANGLE
	RIGHT OF WAY KEY	XX" (YYY)	DISTANCE NOTES IN INCHES (MILLIMETERS)
		SYDL	SOLID DOUBLE YELLOW LINE
(A)	RIGHT TO GRADE	VGC	VERTICAL GRANITE CURB

RIGHT TO RECONSTRUCT DRIVEWAY, STEPS, AND/OR WALK

EASEMENT TO CONSTRUCT DRAINAGE

ROADWAY RIGHT-OF-WAY TAKING

PARCEL NUMBER

ROVE REWSTER S NECTICUT OCK APE
/ENUE A
ieport, **4** $\mathbf{\Omega}$ TSC ELD AN BRIDG

THE CITY OF BRIDGEPORT 999 BROAD STREET BRIDGEPORT, CT

FREEMAN

36 JOHN STREET, HARTFORD, CT 06106 WWW.FREEMANCOS.COM

(860)251-9550

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RZ Design Associates, Inc

DRAFTED: CHECKED: APPROVED:

K.M. R.G. PROJECT NO.:

2016-0238 08/06/201 IN-1 & GN-

GENERAL NOTES

SURVEY NOTES

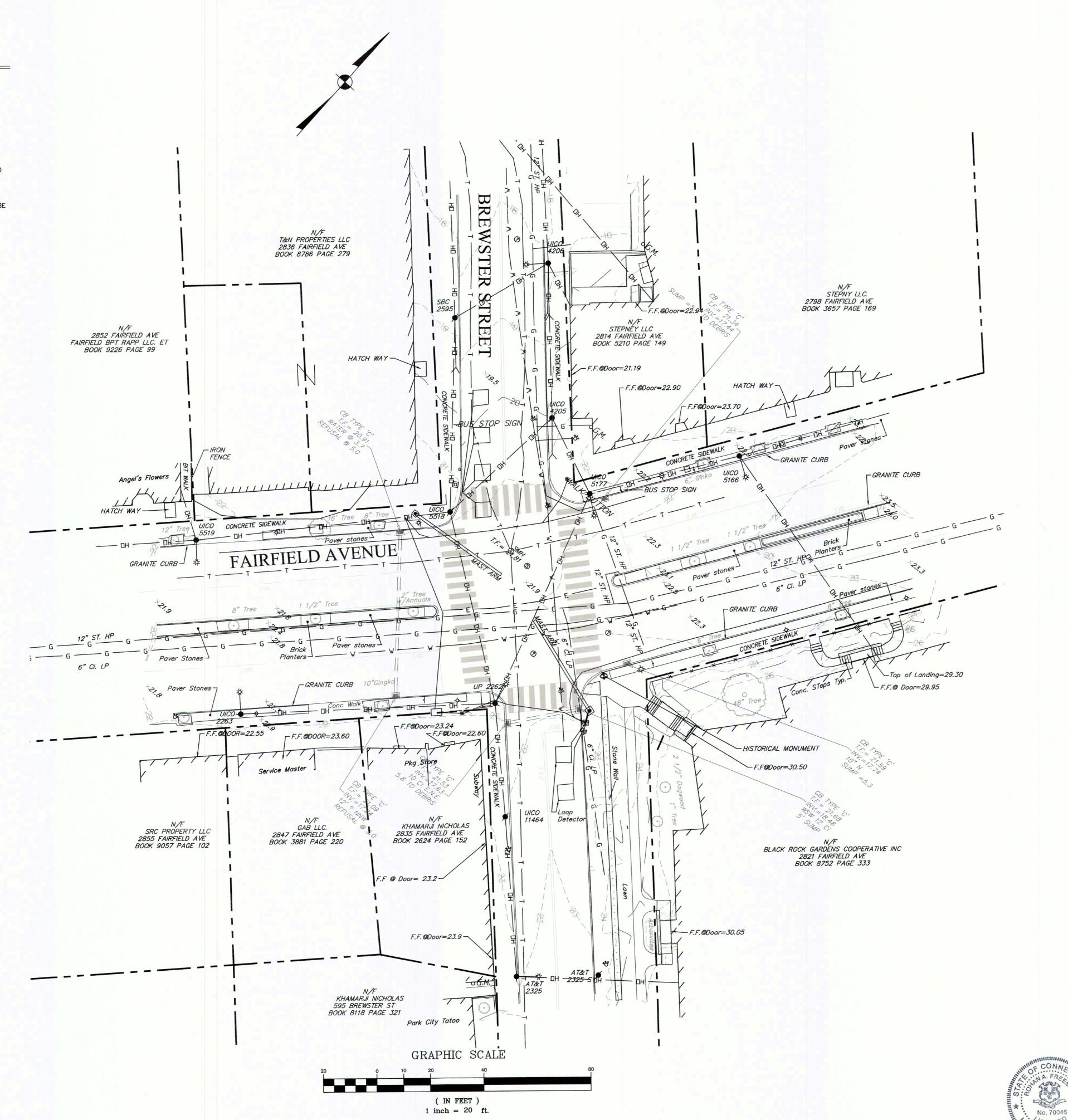
THIS SURVEY HAS BEEN PREPARED BY FREEMAN COMPANIES, LLC. IN ACCORDANCE WITH THE REGULATIONS OF CONNECTICUT STATE AGENCIES, SECTIONS 20-300b-1 THRU 20-300b-20 AND THE "STANDARDS FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT" ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. ON SEPTEMBER 26,

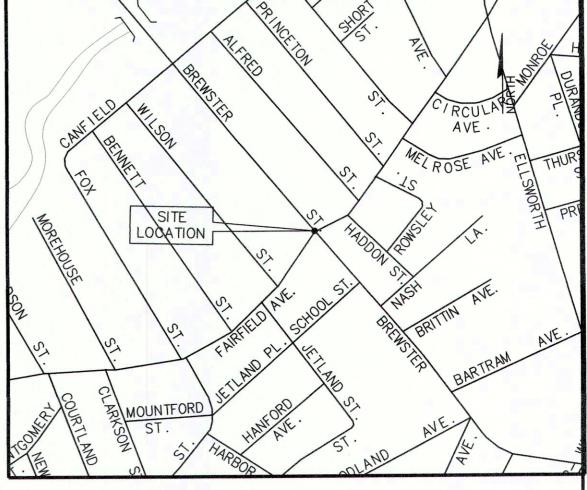
THE TYPE OF SURVEY IS A BOUNDARY AND TOPOGRAPHIC SURVEY. IT IS DEPENDENT IN NATURE AND BASED UPON MAP REFERENCE (BLOCK MAP CITY OF BRIDGEPORT 110 SCALE 1"=50)

THE SURVEY ACCURACY CONFORMS TO HORIZONTAL CLASS "C", TOPOGRAPHIC ACCURACY CLASS T-2. THE VERTICAL DATUM IS CTGS NAVD

THE UNDERGROUND FEATURES DEPICTED HEREON ARE THE RESULT OF COMPILATION OF EXISTING MAPPING AND LOCATION OF UTILITY PAINT. ACTUAL LOCATION OF UNDERGROUND UTILITIES IS TO BE CONSIDERED TO BE APPROXIMATE AT BEST. OTHER UTILITIES MAY EXIST WHICH FREEMAN COMPANIES ARE UNAWARE OF. VERIFY INFORMATION IN THE FIELD. BEFORE ANY DIGGING OR SITE EXCAVATION CALL " CALL BEFORE YOU DIG" 1-800-922-4455.

THESE DRAWINGS SHALL NOT BE UTILIZED BY ANY PERSON, FIRM OR CORPORATION WITHOUT THE SPECIFIC WRITTEN PERMISSION OF FREEMAN COMPANIES, LLC





LOCATION MAP

1"=500

LEGEND

	PROPERTY LINE
	EASEMENT LINE
	CHAIN LINK FENCE
	EDGE OF WETLANDS
	EDGE OF RIVER
— w — — w —	WATER LINE
s s	SANITARY LINE
— G —— G ——	GAS LINE
	OVERHEAD WIRES
	DRAINAGE PIPE
\$L.P.	METAL STREET LIGHT
M.P.	METAL POST
•	UTILITY POLE
W.G.º	WATER GATE VALVE
₽ Hyd.	FIRE HYDRANT
G. G ₆	GAS GATE VALVE
-	SIGN
•	PROPERTY CORNER IRON PIN
	CATCH BASIN
0	DRAINAGE MANHOLE
entities and an analysis of the second secon	SQUARE DRAIN
S	SANITARY MANHOLE
E.O.P.	EDGE OF PAVEMENT
②	UNKNOWN MANHOLE
WF#	WETLAND FLAG
MB	MAILBOX
*	BORING
	BUSH
10"	
	DECIDUOUS TREE
。Bollard	BOLLARD
• M. Well	MONITORING WELL
DWL	DASHED WHITE LINE
SWL	SINGLE WHITE LINE
SYL	SINGLE YELLOW LINE
x 210.02	SPOTGRADE
G.L.R.	GLASTONBURY LAND RECORDS
VOL.	VOLUME

PAGE

CERTIFICATION

NO DECLARATION IS EXPRESSED OR IMPLIED BY THIS MAP OR COPIES THEREOF UNLESS IT BEARS THE IMPRESSION TYPE SEAL AND ORIGINAL LIVE SIGNATURE OF THE SURVEYOR WHOSE NAME AND REGISTRATION NUMBER APPEAR BELOW. ANY CHANGES MADE TO THIS PLAN WITHOUT THE KNOWLEDGE OF THE SIGNERS INVALIDATES THESE DECLARATIONS.

TO THE BEST OF MY KNOWLEDGE AND BELIEF THIS MAP IS SUBSTANTIALLY CORRECT AS DEPICTED AND NOTED HEREON.

ROHAN A. FREEMAN L.S. 70046

10/29/18 DATE

City of Bridgeport City of Bridgeport

> FREEMAN COMPANIES, LLC 100 WELLS STREET, SUITE 2H HARTFORD, CT 06103 WWW.FREEMANCOS.COM (860)251-9550 TOLL FREE:(800)604-5141

FREEMAN COMPANIES, LLC

36 JOHN STREET HARTFORD, CT 06106 WWW.FREEMANCOS.COM (860)251-9550 FAX:(860)986-7161

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CK BUSINESS DISTRICT S
IMPROVEMENT PROJEC
FAIRFIELD AVENUE

DRAFTED: R.L.C P.P. APPROVED: 1" = 20'2016-0238 PROJECT NO .: 7-26-17 CAD FILE: 2016-0238-srv

> **EXISTING** CONDITIONS SURVEY

SURVEY NOTES

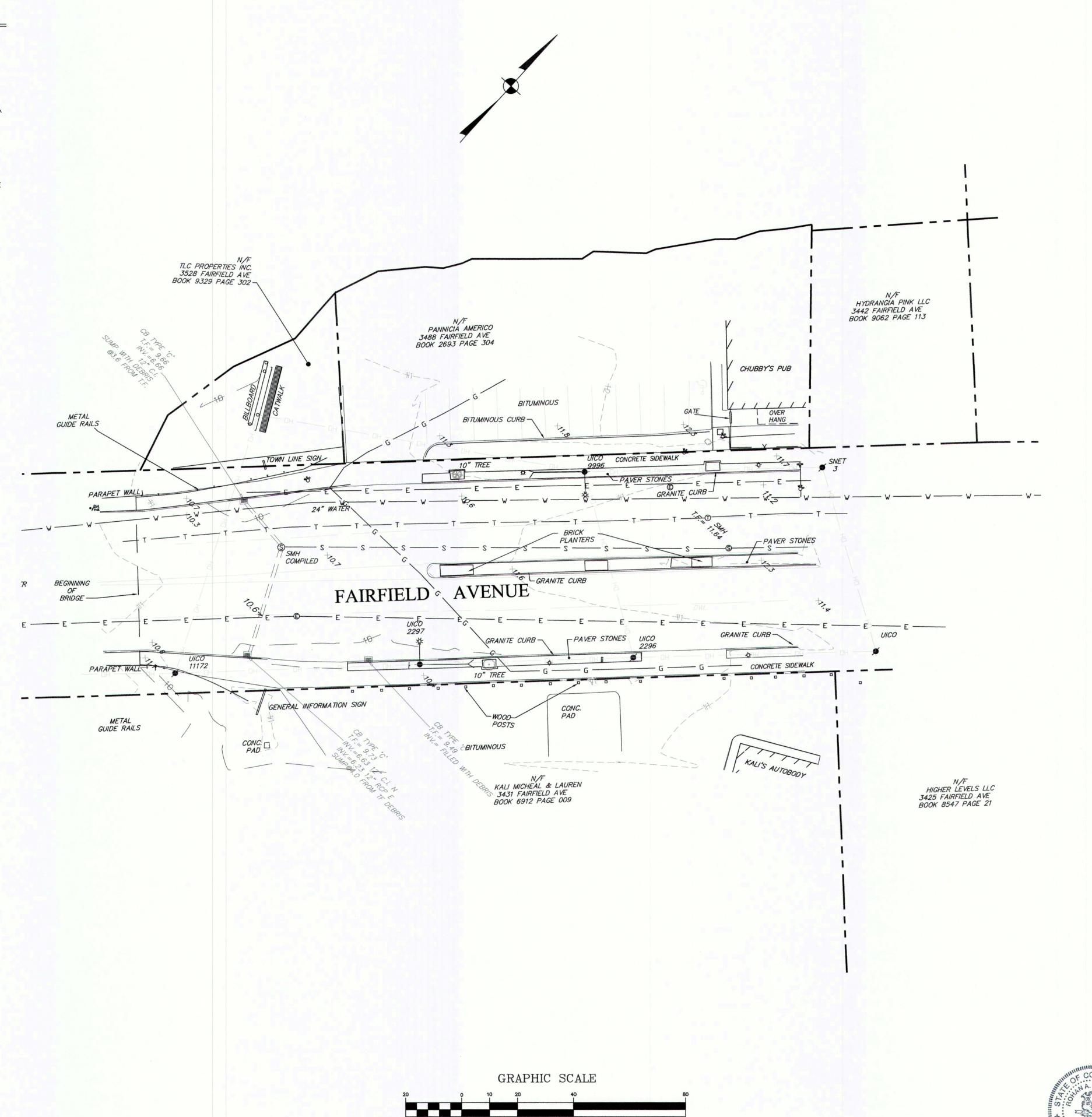
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THE TYPE OF SURVEY IS A BOUNDARY AND TOPOGRAPHIC SURVEY. IT IS DEPENDENT IN NATURE AND BASED UPON FIELD LOCATIONS AND MAP REFERENCE (IMPROVEMENT LOCATION SURVEY PREPARED FOR LAMAR OF .\
WESTERN MASS #3528 FAIRFIELD AVENUE BRIDGEPORT ,CONNECTICUT SCALE 1"=10' APRIL 30, 2015)

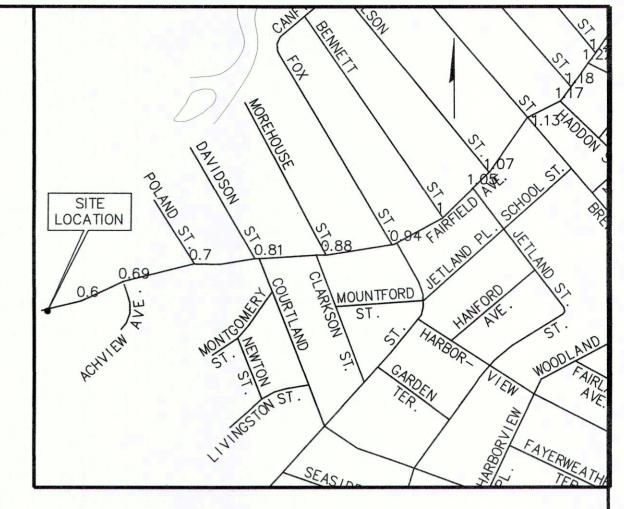
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(IN FEET) 1 inch = 20 ft.



LOCATION MAP

1'=500

LEGEND

LEGEND				
	PROPERTY LINE			
	EASEMENT LINE			
	CHAIN LINK FENCE			
	EDGE OF WETLANDS			
	EDGE OF RIVER			
w	WATER LINE			
s s	SANITARY LINE			
G G	GAS LINE			
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G.L.R.	GLASTONBURY LAND RECORDS			

CERTIFICATION

VOL. PG.

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VOLUME

PAGE

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ROHAN A. FREEMAN L.S. 70046

10 24 12

FREEMAN COMPANIES, LLC 100 WELLS STREET, SUITE 2H HARTFORD, CT 06103 FAX:(860)986-7161

City of Bridgeport

City of Bridgeport

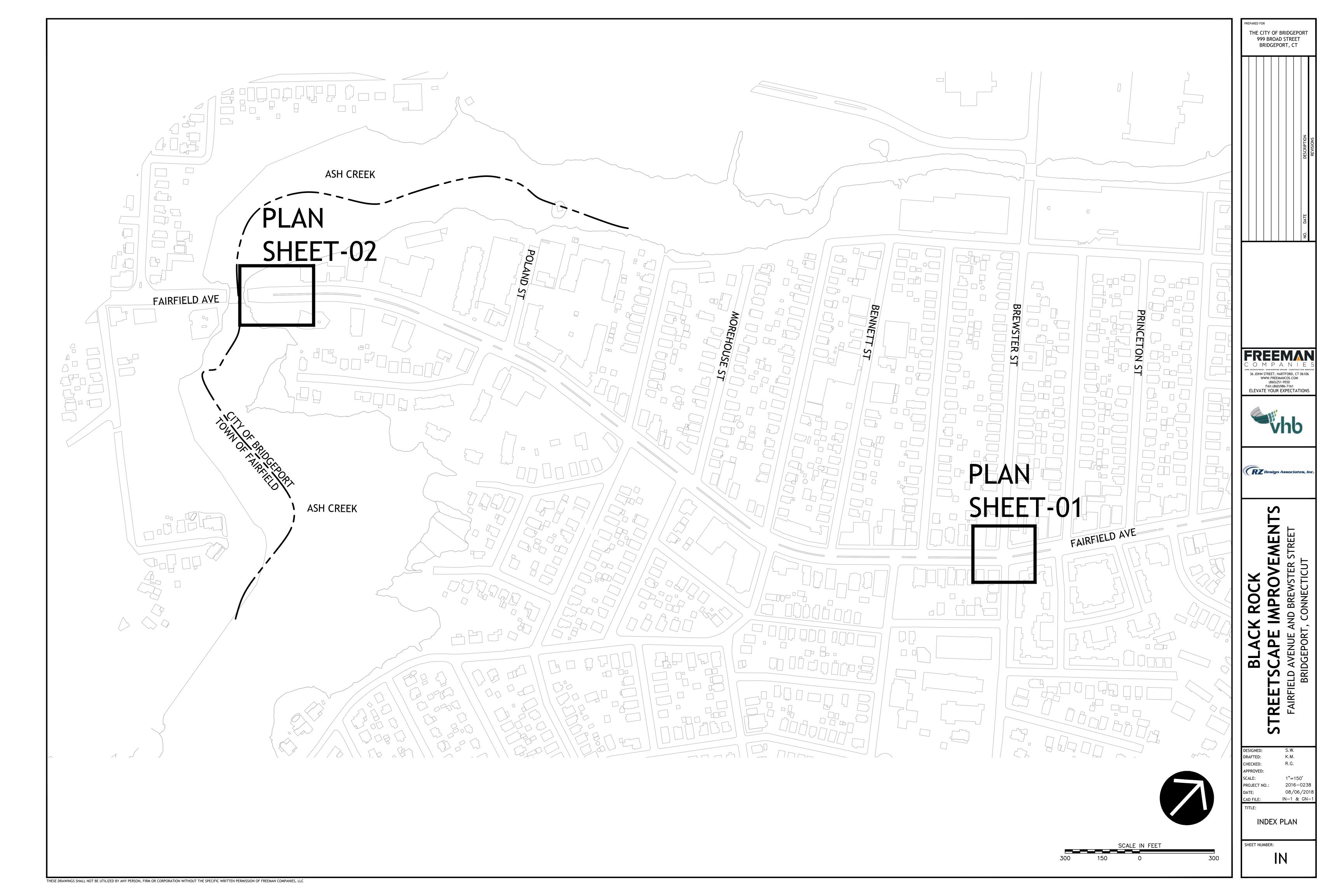
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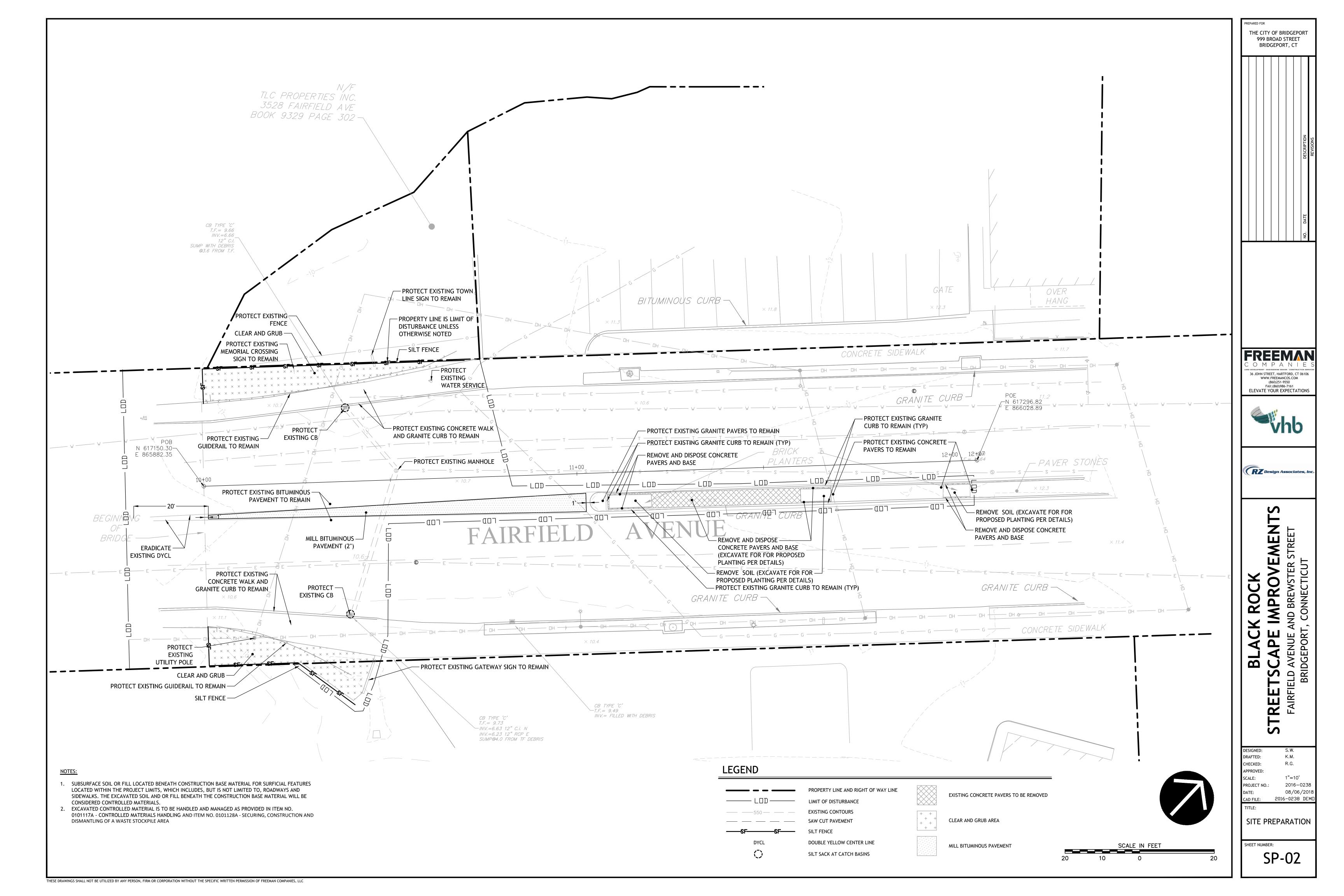
DBE | DAS | MBE | GNMSDC CERTIFIED CIVIL | GEOTECHNICAL SURVEY | ENVIRONMENTAL

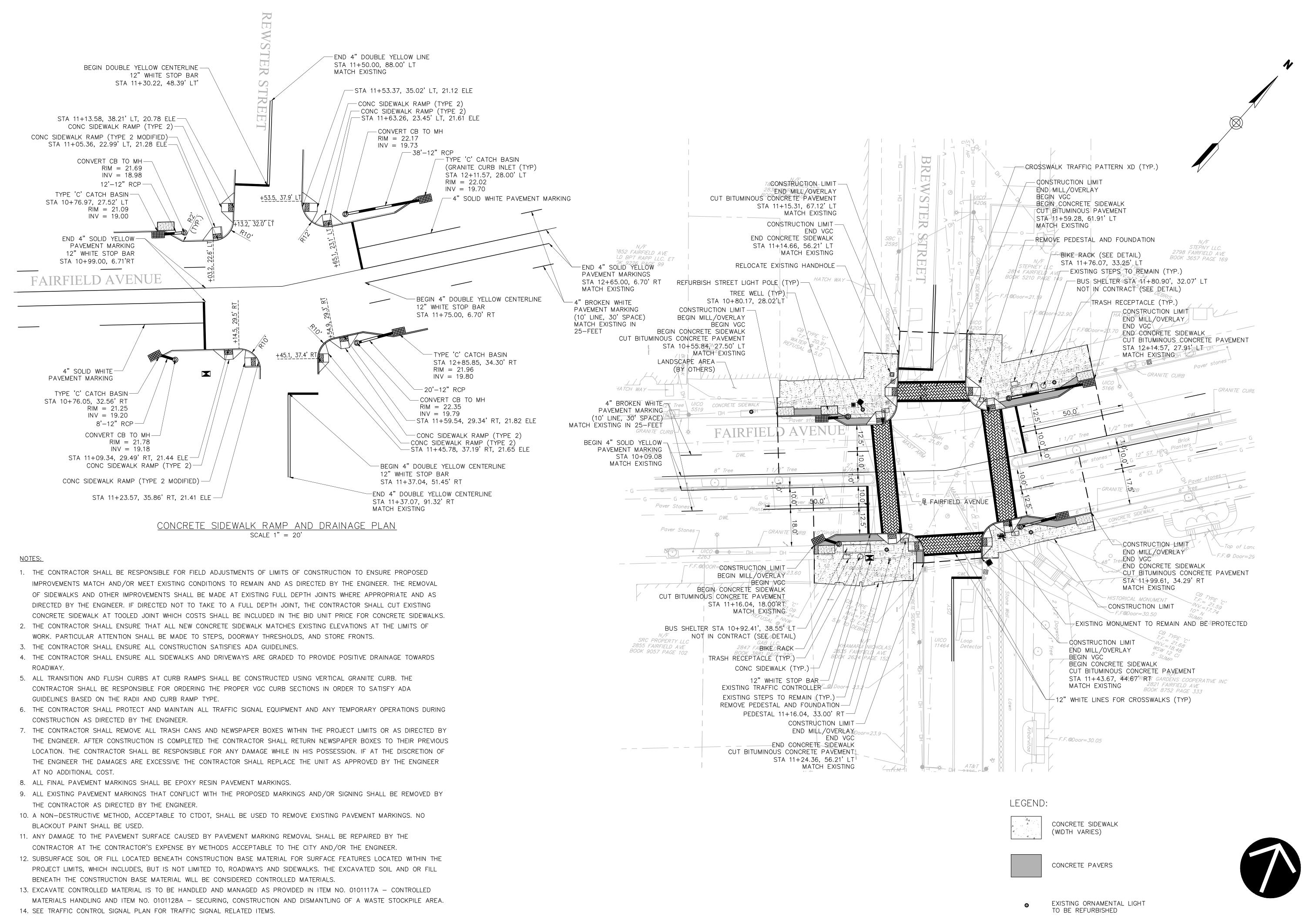
K ROCK BUSINESS DISTRICT STREETS
IMPROVEMENT PROJECT
FAIRFIELD AVENUE
BRIDGEPORT, CONNECTICUT

DRAFTED: R.L.C P.P. APPROVED: SCALE: 1'' = 20'PROJECT NO .: 2016-0238 7-26-17 CAD FILE: 2016-0238-srv

EXISTING CONDITIONS SURVEY







THE CITY OF BRIDGEPORT 999 BROAD STREET BRIDGEPORT, CT FREEMAN 36 JOHN STREET, HARTFORD, CT 06106 WWW.FREEMANCOS.COM (860)251-9550 FAX:(860)986-7161 **ELEVATE YOUR EXPECTATIONS RZ** Design Associates, Inc.

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S

CHECKED: APPROVED:

PROJECT NO.: 2016-0238

08/06/2018 TITLE:

CONSTRUCTION PLAN

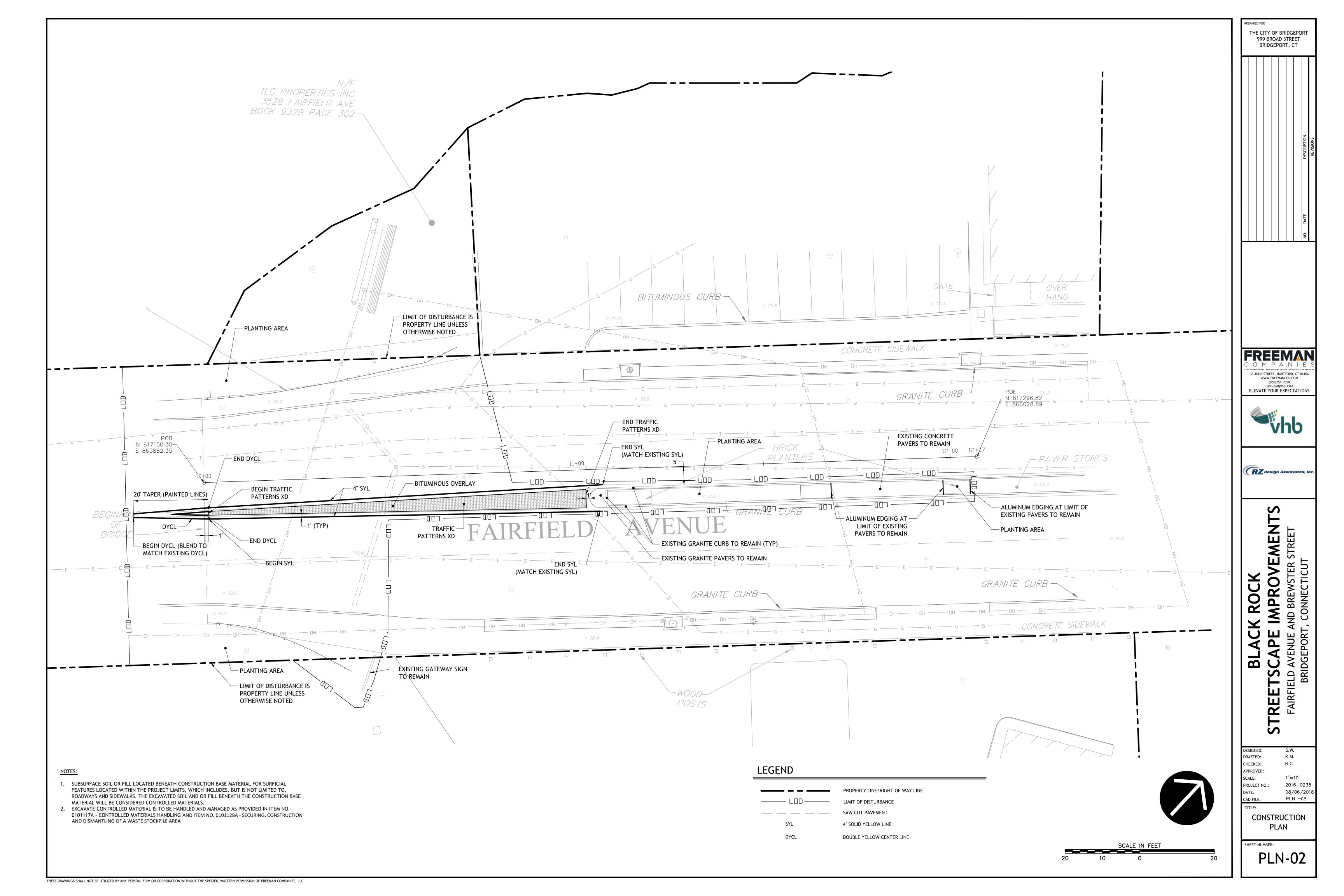
HEET NUMBER:

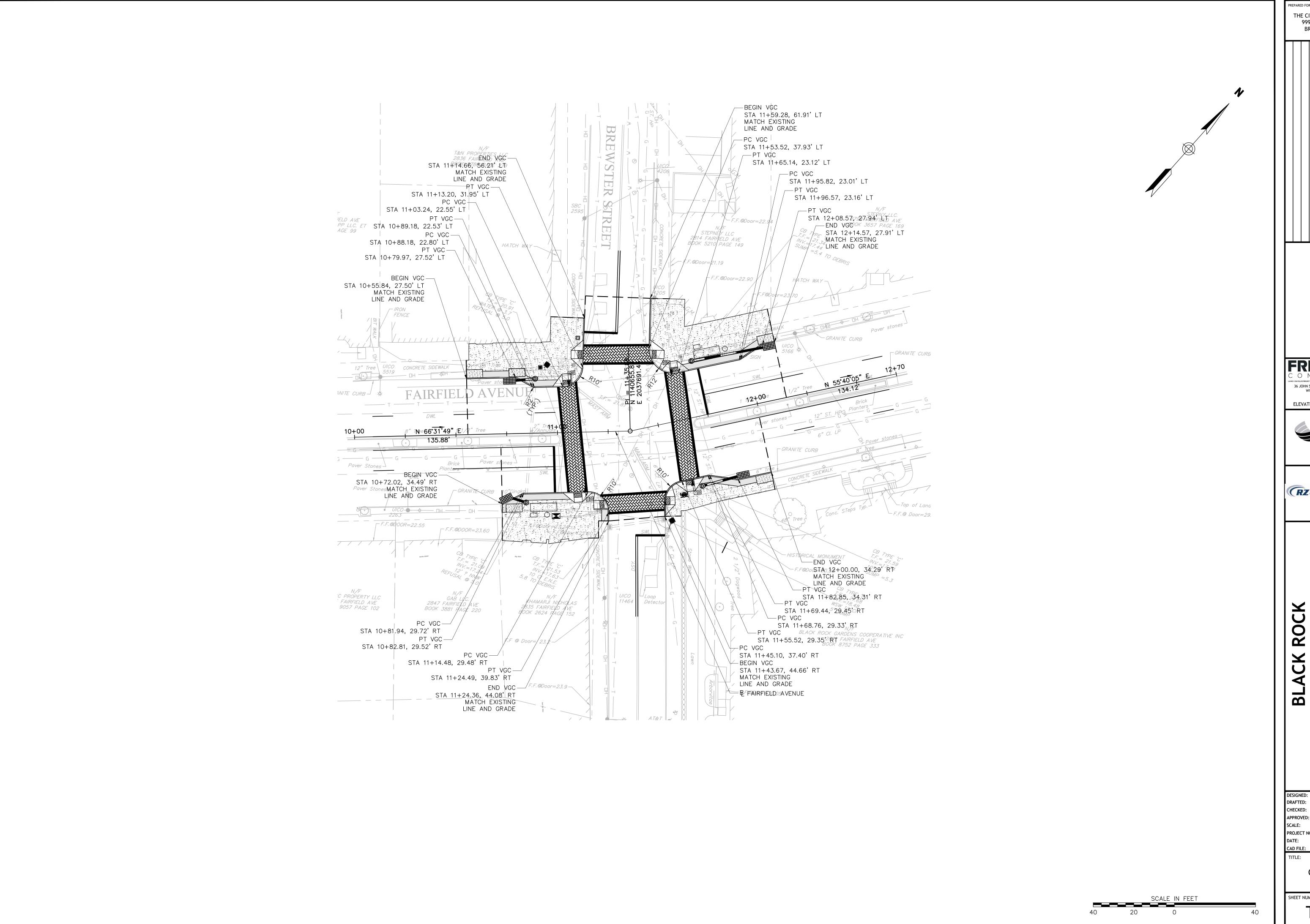
PLN-01

REPLACEMENTS ACCORDINGLY.

15. ALL WORK SHALL CONFORM TO STATE OF CONNECTICUT STANDARD SPECIFICATIONS FORM 817 AND SUPPLEMENTS.

16. ALL BUILDING FRONTAGE STEPS TO REMAIN AT ALL INTERSECTION CORNERS. CONTRACTOR SHALL GRADE ADJACENT SIDEWALK





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THE CITY OF BRIDGEPORT 999 BROAD STREET BRIDGEPORT, CT

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36 JOHN STREET, HARTFORD, CT 06106

WWW.FREEMANCOS.COM

(860)251-9550

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ELEVATE YOUR EXPECTATIONS









IMPROVEMENT
AND BREWSTER STREET
CONNECTICUT

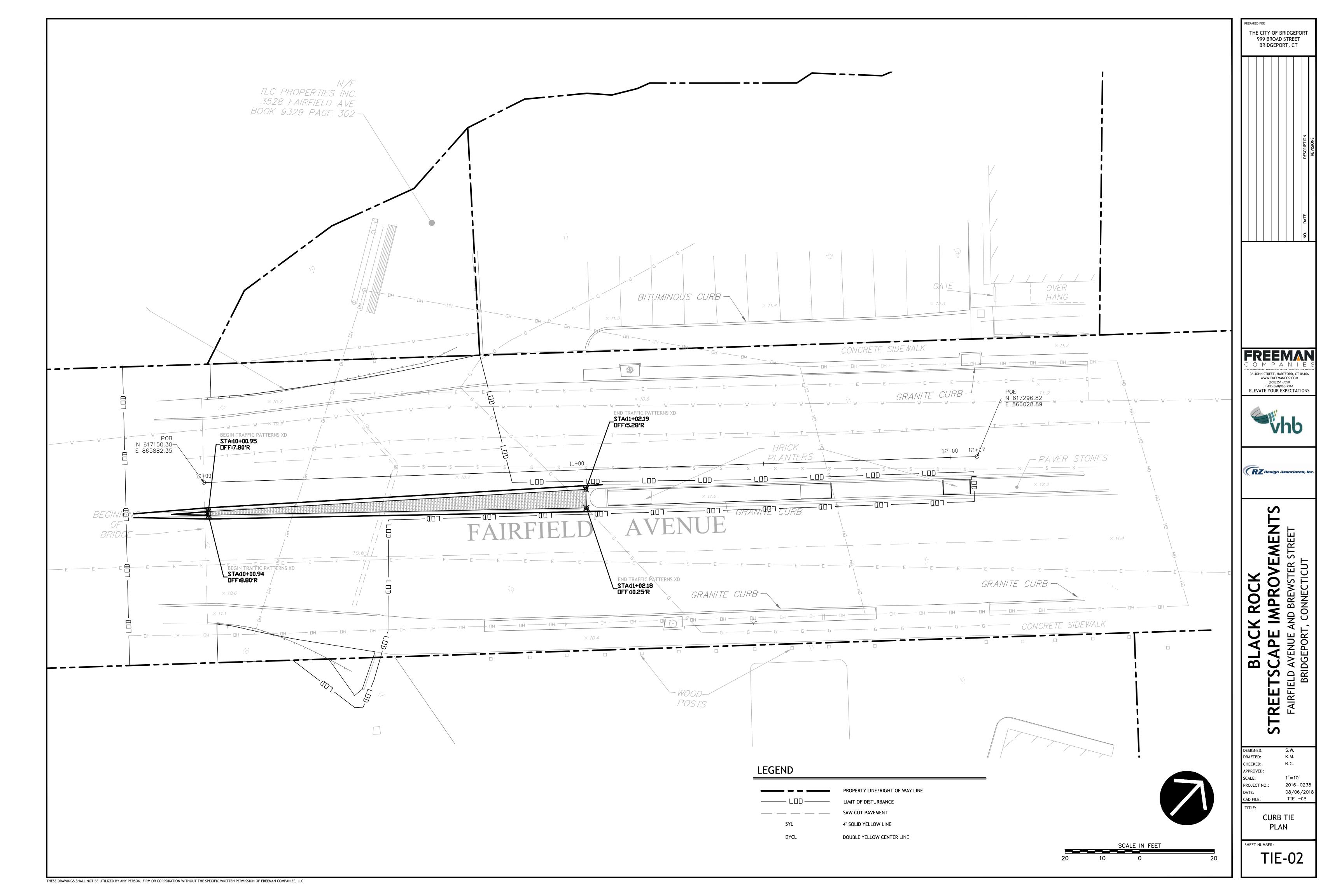
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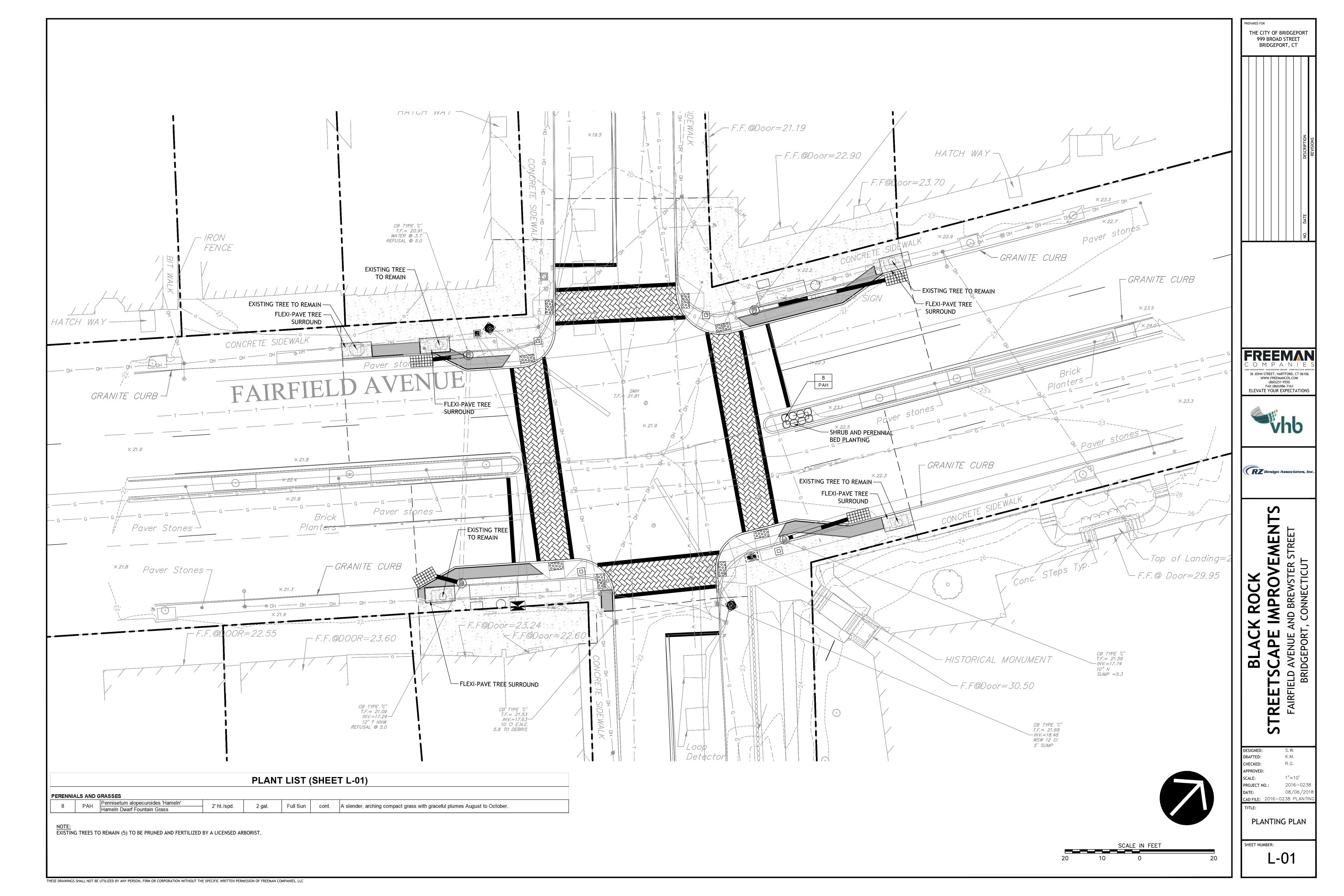
2016-0238 08/06/2018

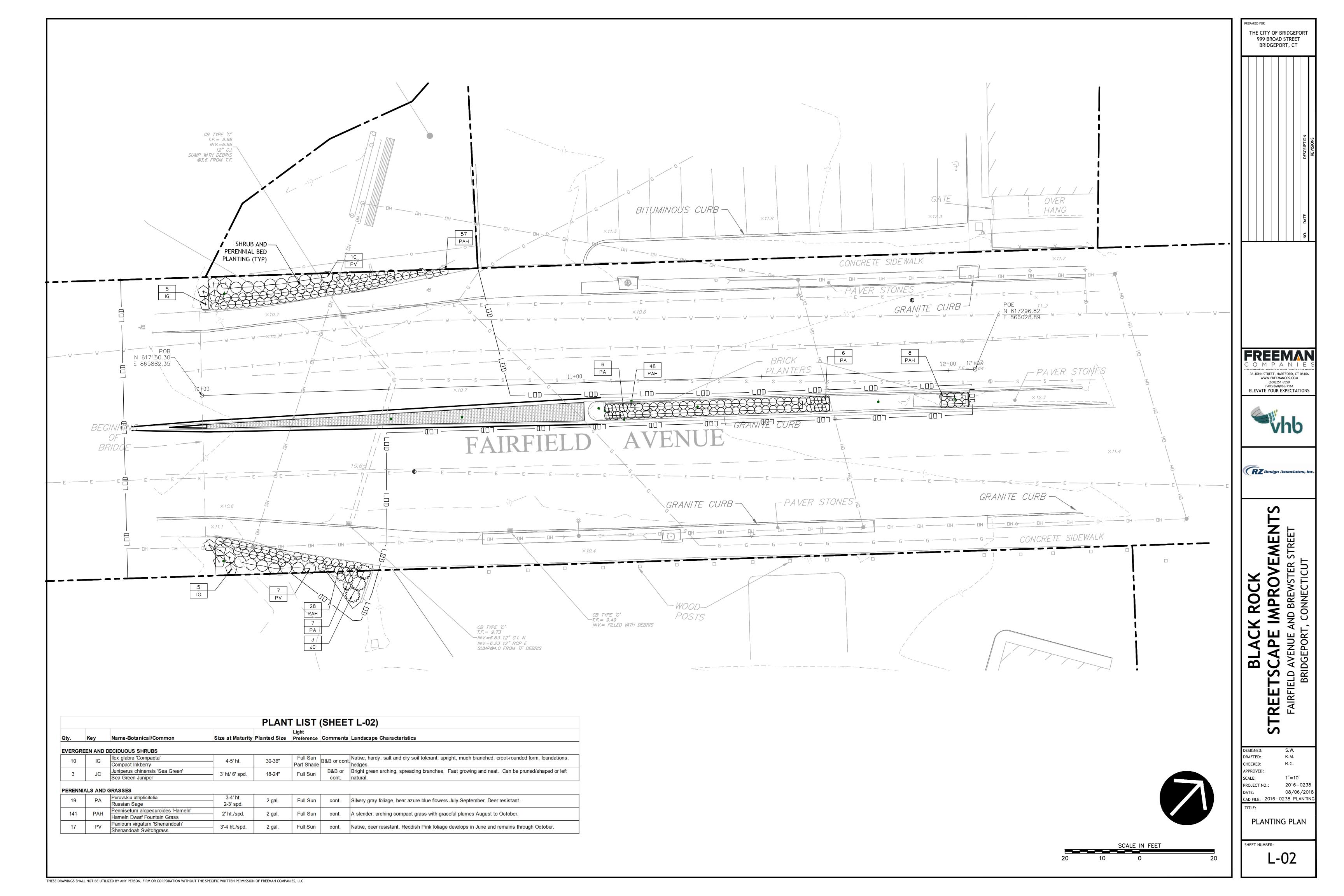
CURB TIE PLAN

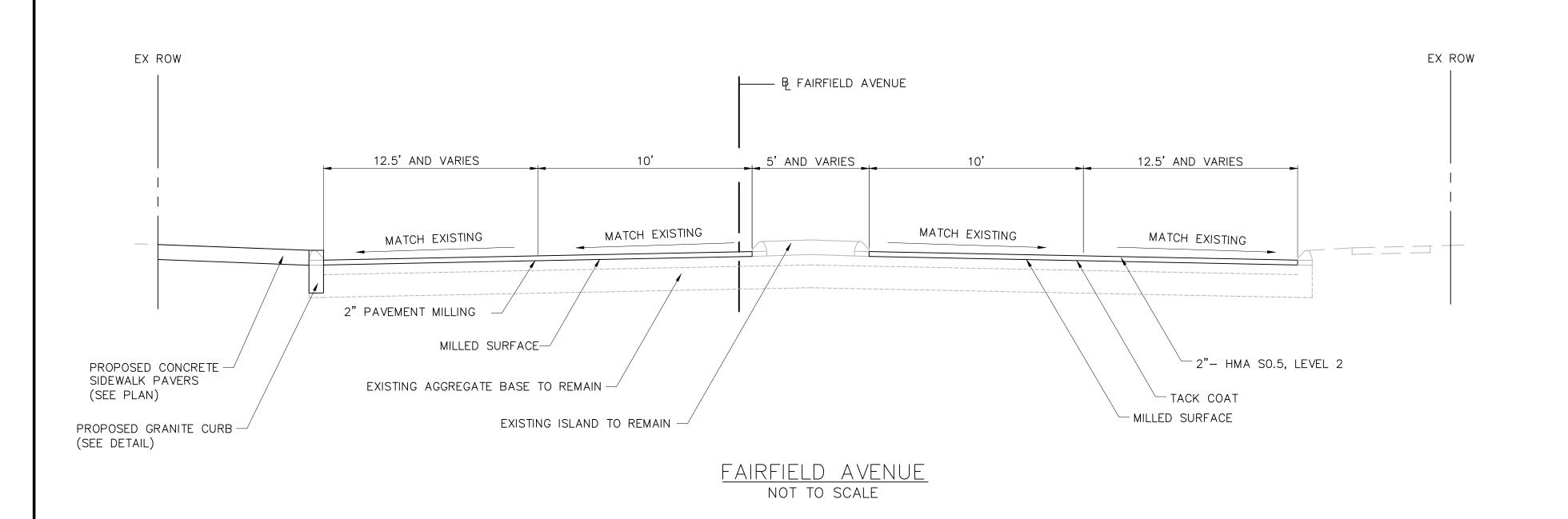
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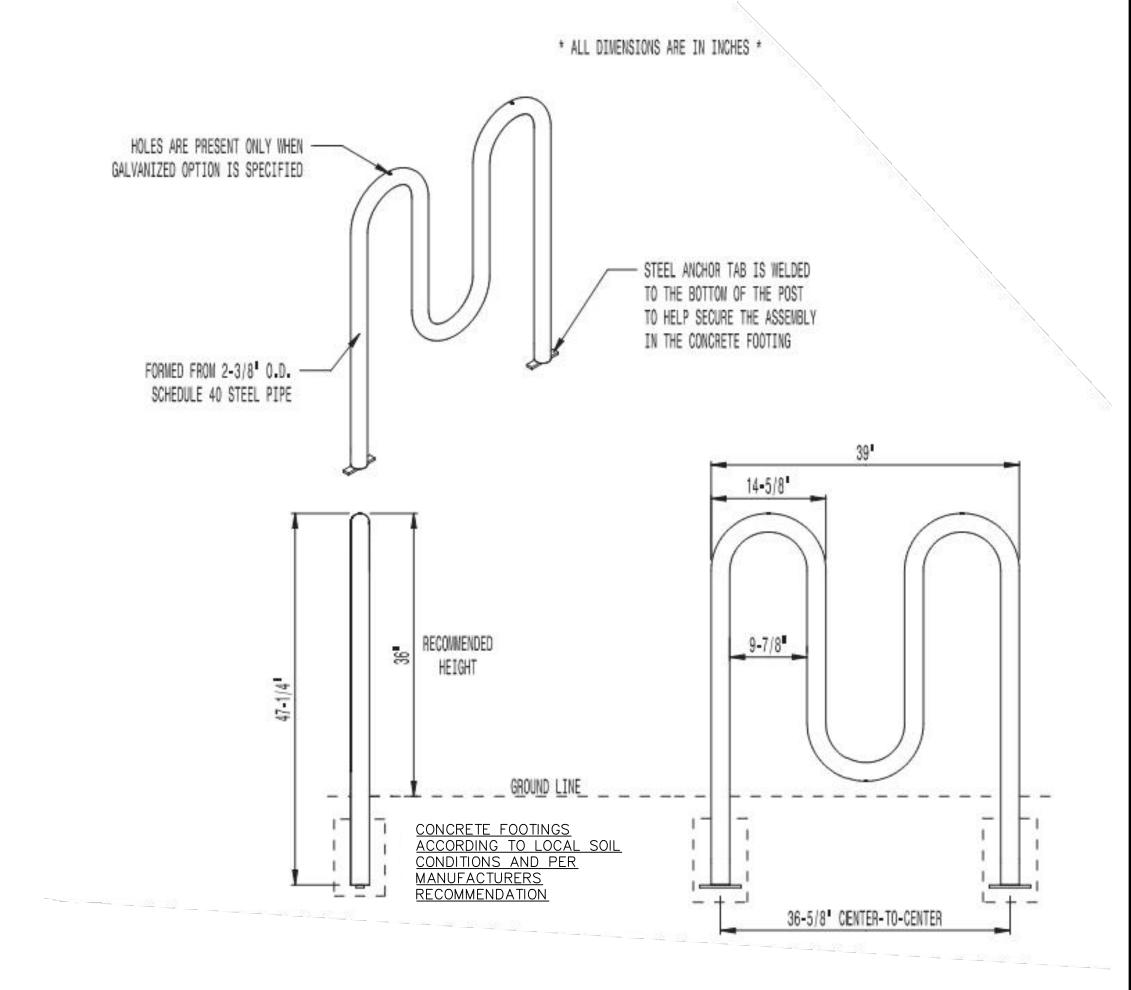








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NOTES:

- 1. ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PREHEATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD COATING FILM. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
- 2. THIS SECURE SITE DESIGNS, LLC. PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND.
- CONSULT YOUR LOCAL CODES FOR THE REGULATIONS. 3. THE COLOR FOR THIS PRODUCT SHALL BE BLACK.

<u>Bike rack detail — in ground mount</u> NOT TO SCALE

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E IMPROVEMENT E AND BREWSTER STREET ST, CONNECTICUT

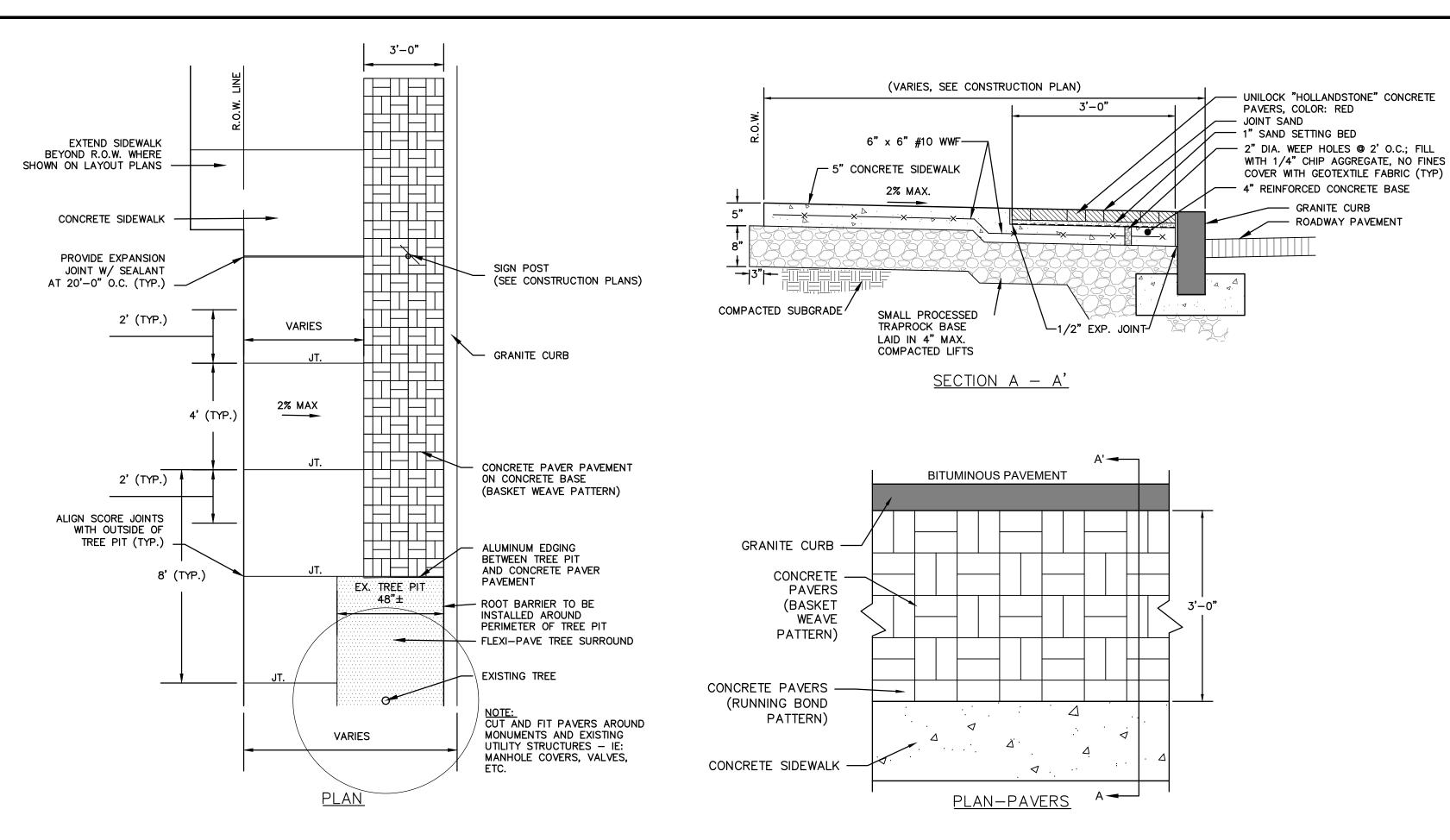
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CHECKED: APPROVED: SCALE: PROJECT NO.: 2016-0238 08/06/2018

> MISCELLANEOUS **DETAILS**

SHEET NUMBER:

MDS-01

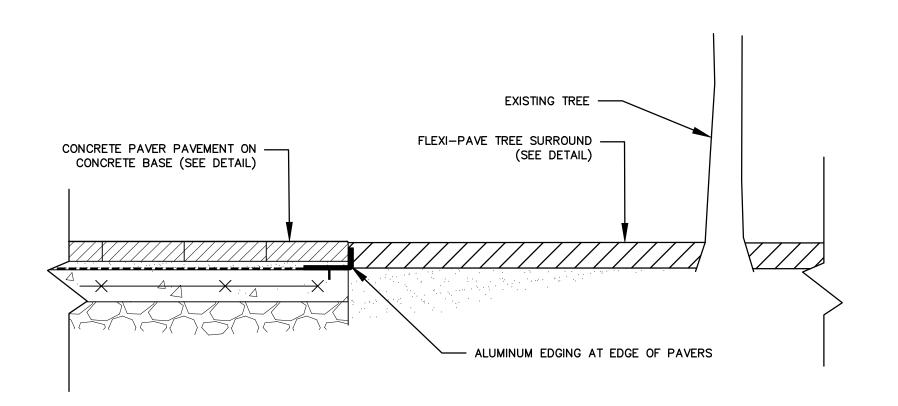


NOTES:

- 1. SAW CUT PAVERS TO MATCH FLUSH AGAINST FIXED OBJECTS, CONCRETE PAVEMENT, EXPOSED UTILITY STRUCTURES AND OTHER SURFACE
- FEATURES AND INSTALL EXPANSION JOINT AS SHOWN IN DETAIL.

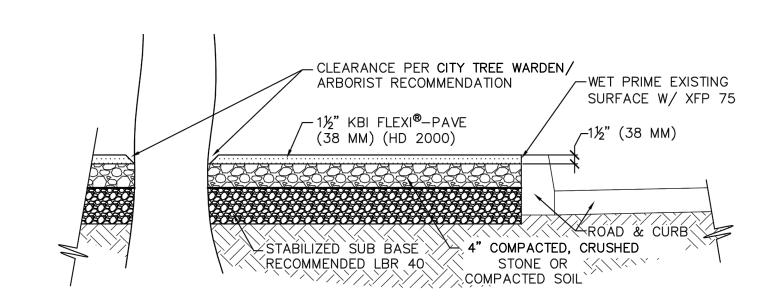
 2. IN LOCATIONS WHERE PAVERS ABUT A TREE PIT OR LANDSCAPED AREA, ALUMINUM EDGING SHALL BE INSTALLED. 3. THE CONTRACTOR SHALL ADHERE TO TYPICAL/MAXIMUM GRADES INDICATED, EXCEPT WHERE NOTED OTHERWISE AND AS DIRECTED BY
- 4. FOR CONCRETE PAVER PAVEMENT LIMITS, SEE CONSTRUCTION PLAN SHEETS.
 5. THE CONTRACTOR SHALL CONSTRUCT TRANSVERSE JOINTS IN THE CONCRETE SIDEWALK AND 4" CONCRETE BASE SLAB AS FOLLOWS, EXCEPT AS NOTED OTHERWISE OR AS DIRECTED BY THE ENGINEER: • AT 5' O.C. PROVIDE 1" DEPTH TOOLED JOINTS
- •AT 15' O.C. PROVIDE 1" PREFORMED EXPANSION JOINTS WITH 1"X24" DOWELS PLACED 24" O.C.
 6. DETAILED INFORMATION REGARDING THE COLOR AND MATERIAL TYPE OF THE CONCRETE PAVERS CAN BE FOUND IN THE SPECIAL PROVISIONS. DETAILED INFORMATION REGARDING THE PAVER PATTERN CAN BE FOUND ON THE DETAIL SHEETS.

CONCRETE PAVER PAVEMENT ON CONC. BASE



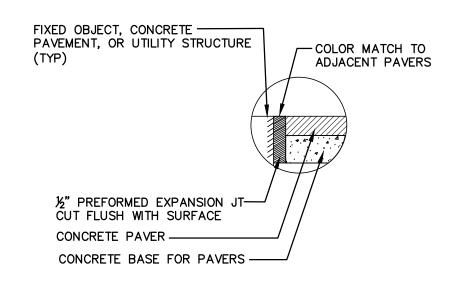
1. EXISTING TREES TO REMAIN TO BE PRUNED AND FERTILIZED BY A LICENSED ARBORIST. 2. INSTALL ALUMINUM EDGING PER MANUFACTURES INSTRUCTIONS

TREE PIT



DUE TO VARIANCES IN LOCAL CODES, CONSTRUCTION PRACT DETAILS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SU PRACTICES AND REQUIREMENTS. REGARDLESS OF DETAIL CO KB INDUSTRIES, INC. RESERVES THE RIGHT TO CHANGE SPE NOTICE. ALL CHANGES TO SPECIFICATIONS CAN ONLY BE AS	CH LOCAL CODES, CONSTRUCTION NSTRUCTION SHOWN. CIFICATIONS SHOWN WITHOUT	ANY WARRANTY GIVEN BY KB INDUSTRIES WILL APPLY ONLY TO THOSE ITEMS SUPPLIED BY IT. ACC CANNOT ACCEPT ANY RESPONSIBILITY FOR ANY STRUCTURE OR EQUIPMENT DESIGNED BY OTHERS. K A POLICY OF CONTINUOUS IMPROVEMENT AND RESERVE THE RIGHTS THE AMEND ITS PRODUCT SPEC DRAWINGS. KB INDUSTRIES CANNOT ACCEPT LIABILITY FOR THE PERFORMANCE OF ITS PRODUCTS IF SUBJECT TO CONDITIONS OUTSIDE ITS DESIGN SPECIFICATIONS.	(B INDUSTRIES HAS IFICATIONS AND
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	K.B.Ind	ustries, Inc.	DRAWN BY-TJG DRAWING
	7300 BRYAN DA	AIRY RD., SUITE 400 LARGO, FL.	HD2000 17
	PH: 727-723-33	·	02/09/15 SCALE
	www.kBius.com	М	3/4"= 1'-0"

FLEXI-PAVE TREE SURROUND



EXPANSION JOINT

- TOP OF SHRUB ROOT BALL 1 INCH ABOVE FINISH GRADE

- 3" PINE BARK MULCH DO NOT COVER STEMS OR TRUNK - UNTIE AND ROLL BACK BURLAP FROM 1/3 (MIN) OF ROOT BALL; IF SYNTHETIC WRAP IS USED, REMOVAL COMPLETELY

- SLOPE TO FORM SAUCER - PLANTING SOIL

SIT ROOTBALL ON EXISTING UNDISTURBED SOIL OR ON COMPACTED SUBGRADE

1. LOOSEN ROOTS AT THE OUTER EDGE OF ROOTBALL OF CONTAINER GROWN SHRUBS.

EXCAVATE SHRUB & PERENNIAL BED TO -

REQUIRED DEPTH AND BACKFILL WITH SPECIFIED

PLANTING SOIL. SOIL SHALL BE CONTINUOUS

SHRUB AND PERENNIAL BED PLANTING

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DRAFTED: K.M. CHECKED: APPROVED: PROJECT NO.: 2016-0238

CAD FILE: 2016-0238 DETAILS

MISC. DETAILS

MDS-02

EROSION AND SEDIMENT CONTROL NARRATIVE FOR:

BLACK ROCK STREETSCAPE IMPROVEMENTS
FAIRFIELD AVENUE

BRIDGEPORT, CONNECTICUT

I. INTRODUCTION

THE EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED AS PART OF THE CONSTRUCTION PLANS FOR THE CONSTRUCTION OF STREETSCAPE IMPROVEMENTS ON FAIRFIELD AVENUE IN BRIGEPORT, CONNECTICUT. INFORMATION RELATING TO SEDIMENTATION AND EROSION CONTROL IS INCLUDED IN THESE DRAWINGS. ALL SEDIMENTATION AND EROSION CONTROL ACTIVITIES SHALL BE IN COMPLIANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN PREPARED FOR THIS PROJECT.

II. NARRATIVE

A. DESCRIPTION OF DEVELOPMENT

THE STREETSCAPE IMPROVEMENT SITES ARE AT TWO DIFFERENT LOCATIONS ALONG FAIRFIELD AVENUE, BRIDGEPORT, CONNECTICUT. ONE IS NEAR THE ASH CREEK BRIDGE. THE OTHER SITE IS AT THE INTERSECTION OF FAIRFIELD AVENUE AND BREWSTER STREET. THE PROJECT INCLUDES EXTERIOR IMPROVEMENTS SUCH AS DRAINAGE, GRADING, LANDSCAPING, PAVEMENT, AND LIGHT REFINISHING.

B. CONSTRUCTION AND GRADING SCHEDULE

1. GENERA

A. THE FOLLOWING SCHEDULE IS TO SERVE AS A GENERAL GUIDE TO THE SEQUENCE OF CONSTRUCTION ACTIVITIES FOR SEDIMENT AND EROSION CONTROL MEASURES. IT IS NOT INTENDED TO TAKE THE PLACE OF THE CONTRACTOR'S RESPONSIBILITY FOR DETAILED SCHEDULING OF ALL CONSTRUCTION ACTIVITIES. HOWEVER, THE SCHEDULE WILL BE INCORPORATED INTO THE CONTRACT DOCUMENTS AND NO SUBSTANTIAL DEVIATION FROM THIS SCHEDULE SHALL OCCUR WITHOUT PRIOR APPROVAL OF THE CONSTRUCTION MANAGER.

B. THE DATES GIVEN BELOW ARE APPROXIMATE TO GIVE AN INDICATION OF OVERALL CONSTRUCTION SEQUENCE.

FINAL GRADING SPRING 20	START CONSTRUCTION	SPRING	2019
END CONSTRUCTION SPRING 20 THE CONTRACTOR IS REQUIRED TO MEET THE DATES AND DEADLINES ESTABLISHED IN	PARTIAL GRADING	SPRING	2019
THE CONTRACTOR IS REQUIRED TO MEET THE DATES AND DEADLINES ESTABLISHED IN	FINAL GRADING	SPRING	2019
	END CONSTRUCTION	SPRING	2019
CONTRACT EXECUTED WITH THE OWNER.	THE CONTRACTOR IS REQUIRED TO MEET THE DATES AND	DEADLINES ESTABLISHED	IN THEIR
	CONTRACT EXECUTED WITH THE OWNER.		

2. CONSTRUCTION SEQUENCE

THERE ARE THREE MAJOR PHASES OF CONSTRUCTION. EACH PHASE WILL INCLUDE THE INSTALLATION OF EROSION AND SEDIMENT (E&S) CONTROLS WHICH WILL NEED TO BE ADJUSTED AS THE PHASE EVOLVES AND AS DIRECTED BY THE ENGINEER. THE E&S CONTROLS WILL NEED TO BE INSPECTED DURING THE START OF EACH PHASE AND ADJUSTED OR REPLACED AS NEEDED. THE FOLLOWING ARE

- THE PROJECT PHASES;

 1. DEMOLITION (SITE PREP) PHASE
- 2. MAJOR EARTHWORK PHASE3. SITE IMPROVEMENTS PHASE
- A. CLEARING AND GRUBBING

(1.) CLEAR ALL TREES, BRUSH, AND APPURTENANCES WITHIN THE PROPOSED PROJECT AREA THAT ARE NOT DESIGNATED TO REMAIN. CONTRACTOR SHALL VERIFY WITH THE ENGINEER REMOVAL OF ALL TREES. DISPOSE OF CLEARED ITEMS AT AN APPROVED OFF—SITE DISPOSAL AREA.

(2.)CLEAN ALL EXISTING DRAINAGE STRUCTURES THAT ARE CURRENTLY FILLED.

B. EROSION AND SEDIMENT CONTROL

(1.)INSTALL HAYBALES, SILT FENCE, AND SILT SACKS AS SHOWN ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER.

C. SITE EXCAVATION AND GRADING

(1.)STRIP AND STOCK TOPSOIL. INSTALL HAY BALES AND SILT FENCE AROUND STOCKPILE AS REQUIRED.

(2.)RELOCATE OR INSTALL ADDITIONAL SILT FENCE OR HAY BALES TO FULLY ENCLOSE AND CONTROL ALL WORK AREAS AS DIRECTED BY THE ENGINEER.

(3.)AS EXCAVATION PROGRESSES, PROVIDE TEMPORARY CHANNELS OR BERMS AS NECESSARY TO DIRECT RUNOFF RUNOFF TO THE PROPOSED OR EXISTING DRAINAGE STRUCTURES AS DIRECTED BY

THE ENGINEER. OWNER.

(4.)THE CONTRACTOR SHALL STOCKPILE ALL EXCESS EXCAVATED MATERIAL AT LOCATIONS SHOWN ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER. HAYBALES SHALL BE PLACED AROUND THE PERIMETER OF ALL STOCKPILES. EXCESS MATERIAL WHICH WILL NOT BE REUSED SHALL BE

TAKEN OFFSITE IMMEDIATELY.

(5.)REPLACE CLOGGED SEDIMENTATION CONTROL BALES AS REQUIRED AND CLEAN SEDIMENT FROM BASINS WHEN ACCUMULATION SEDIMENT EXCEEDS 8" IN DEPTH.

D. STORM DRAINAGE STRUCTURES

SILT SACKS.

(1.)AS SOON AS POSSIBLE CONSTRUCT STORM DRAINAGE SYSTEMS ON—SITE AND/OR SILT FENCE AROUND ALL INLETS TO PREVENT SEDIMENT FROM ENTERING NEWLY CONSTRUCTED OR EXISTING DRAINAGE SYSTEMS.

(2.)FOLLOWING CONSTRUCTION OF CATCH BASINS AND OTHER INLETS, PROVIDE HAY BALES AND

E. ROUGH GRADING AND PAVING

SEDIMENT AND EROSION CONTROLS WITHIN THE PAVED AREAS SHALL BE LEFT IN PLACE UNTIL IMMEDIATELY BEFORE PAVING. MEASURES OUTSIDE OF THE PAVED AREA SHALL REMAIN UNTIL A STABLE VEGETATIVE GROWTH HAS BEEN ESTABLISHED ON ALL SLOPES OR UNTIL DIRECTED BY THE ENGINEER.

F. FINAL ITEMS

CLEAN ALL CATCH BASINS AND STORM MANHOLES OF ALL ACCUMULATED SEDIMENT AS DIRECTED BY THE ENGINEER.

3. CONTINGENCY PLANS FOR FAILED EROSION AND SEDIMENTATION

CONTROL MEASURES

FAILED EROSION AND SEDIMENTATION CONTROL MEASURES WILL BE EVALUATED ON A CASE BY CASE BASIS BY THE ENGINEER AND APPROPRIATE MEASURES TAKEN. THESE MEASURES MAY INCLUDE CLEANING AND/OR REPLACEMENT OF DEFECTIVE FACILITIES OR INSTALLATION OF NEW OR SUPPLEMENTAL FACILITIES.

C. DESIGN CRITERIA

THE FOLLOWING DESIGN REFERENCES WERE FOLLOWED FOR THE PREPARATION OF STORM DRAINAGE DESIGN AND EROSION AND SEDIMENT CONTROL PLANS:

- 1. CONNECTICUT DOT DRAINAGE MANUAL
- 2. SCS GUIDELINE FOR SOIL EROSION AND SEDIMENT CONTROL

SEDIMENTATION CONTROL BALES HAVE BEEN DESIGNED IN ACCORDANCE WITH CHAPTER 7, SECTION F OF THE CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL

D. CONSTRUCTION DETAILS

CONSTRUCTION DETAILS FOR THE PROPOSED PROJECT ARE PRESENTED ON THE DETAIL

E. INSTALLATION PROCEDURES

THE INSTALLATION PROCEDURES FOR STORMWATER MANAGEMENT FACILITIES AND EROSION AND SEDIMENTATION CONTROL MEASURES ARE PRESENTED IN THE PROJECTS TECHNICAL SPECIFICATIONS FOR DRAINAGE; EXCAVATION, FILLING AND GRADING; AND SEDIMENTATION AND EROSION CONTROL. ADDITIONAL INSTALLATION PROCEDURES ARE SHOWN ON THE CONSTRUCTION DETAILS BOTH VISUALLY AND BY USE OF CONSTRUCTION NOTES.

F. OPERATION AND MAINTENANCE

1. DURING CONSTRUCTION

UNTIL ALL DISTURBED AREAS ARE STABILIZED.

BEST MANAGEMENT PRACTICES, SHALL BE UTILIZED TO CONTROL STORM WATER DISCHARGES AND TO PREVENT EROSION AND SEDIMENTATION AND TO OTHERWISE PREVENT POLLUTION OF WETLANDS OR WATERCOURSES. FOR INFORMATION AND TECHNICAL ASSISTANCE, CONTACT THE CITY ENGINEER. THE PERMITTEE SHALL IMMEDIATELY INFORM THE ENGINEERING DEPARTMENT OF ANY PROBLEMS INVOLVING WETLANDS OR WATERCOURSES WHICH HAVE DEVELOPED IN THE COURSE OF, OR WHICH ARE CAUSED BY, THE AUTHORIZED WORK. NO EQUIPMENT OR MATERIAL INCLUDING WITHOUT LIMITATION, FILL, CONSTRUCTION MATERIALS,

OR DEBRIS, SHALL BE DEPOSITED, PLACED, OR STORED IN ANY WETLAND OR WATERCOURSE ON OR OFF SITE.

TIMELY IMPLEMENTATION AND MAINTENANCE OF SEDIMENT AND EROSION CONTROL MEASURES

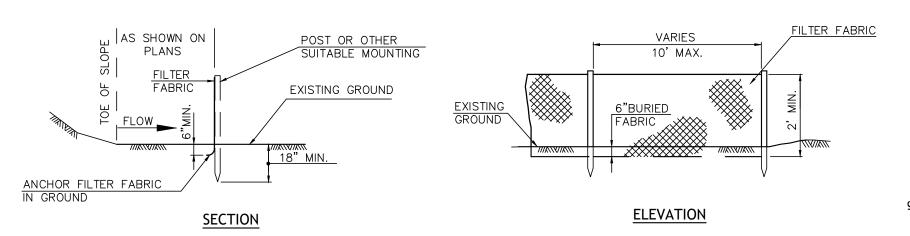
ARE REQUIRED. ALL SEDIMENT AND EROSION CONTROL MEASURES MUST BE MAINTAINED

A PRE-CONSTRUCTION MEETING SHALL BE HELD PRIOR TO THE COMMENCEMENT OF ANY CONSTRUCTION ACTIVITIES ON THE SITE WITH THE CONTRACTOR, AND CITY STAFF.

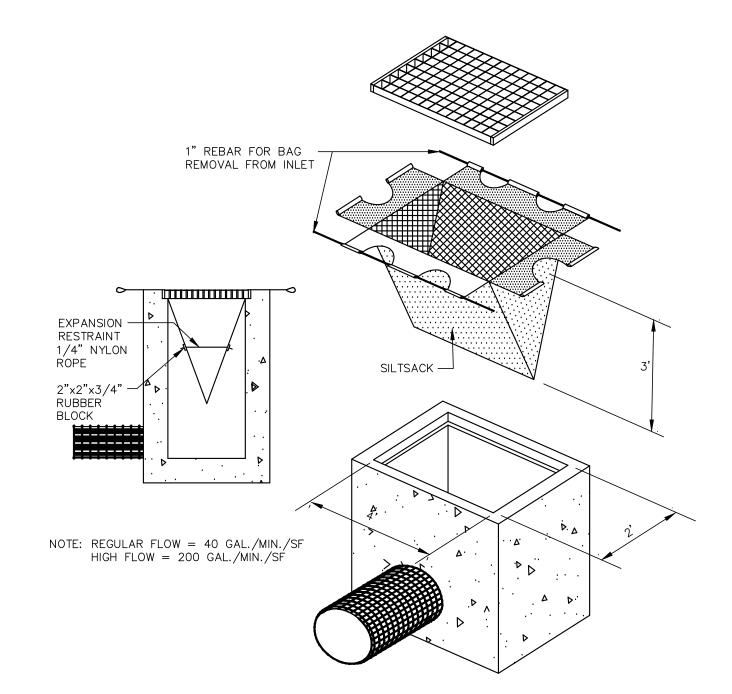
AS CONTAINED IN THE SEDIMENTATION AND EROSION CONTROL SPECIFICATIONS, OPERATIONS AND MAINTENANCE DURING CONSTRUCTION WILL CONSIST OF PERIODIC REPLACEMENT AND/OR CLEANING OF CLOGGED HAY BALES, SILT FENCE AND CONSTRUCTION ENTRANCE AT NO ADDITIONAL COST TO THE OWNER. ANY TEMPORARY SEDIMENTATION BASINS WILL BE CLEANED OF ACCUMULATED SEDIMENT WHEN THE DEPTH OF ACCUMULATED SEDIMENT EXCEEDS 8". ALL DRAINAGE STRUCTURES SHALL BE INSPECTED ON DAILY BASIS AND ANY NECESSARY CORRECTIVE ACTION TAKEN.

G. DESIGNATED ON-SITE AGENT

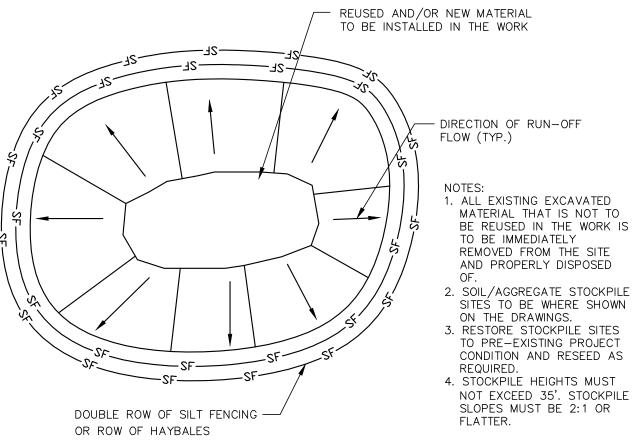
1. THE PERSON RESPONSIBLE FOR OVERSEEING OF PROPER INSTALLATION AND MAINTENANCE OF ALL SOIL EROSION SEDIMENT CONTROL MEASURES FOR THIS PROJECT SHALL BE THE SITE CONTRACTOR.



SILT FENCE

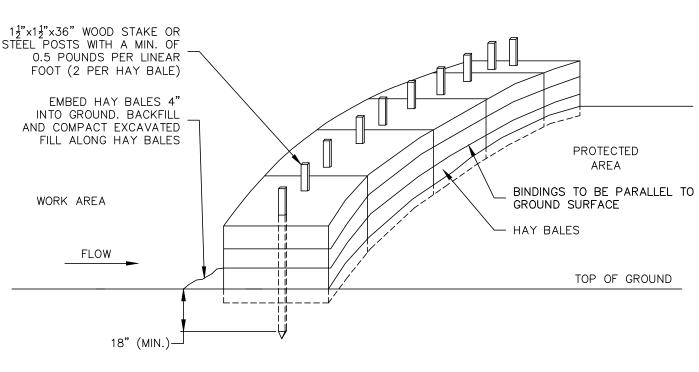


SILTSACK DETAIL



TEMPORARY STOCKPILE AREA

N.T.S.



GENERAL NOTES

- 1. HAY BALES SHALL BE MADE OF HAY OR STRAW WITH 40 POUND MIN. WEIGHT AND 120 POUND MAX. WEIGHT HELD TOGETHER BY TWINE OR WIRE.
- 2. PLACE HAY BALES ON CONTOUR AND WING THE LAST HAY BALES UP SLOPE SO THAT THE TOP OF THE LAST SEVERAL HAY BALES ARE HIGHER THAN THE LINE OF HAY BALES.
- 3. DRIVE FIRST STAKE IN EACH BALE TOWARD THE PREVIOUSLY LAID BALE TO FORCE THEM TOGETHER.
- 4. PUT ONE HAY BALE PERPENDICULAR ALONG HAY BALE BARRIER EACH 100 FEET.

HAYBALE DETAIL

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999 BROAD STREET
BRIDGEPORT, CT

NO.

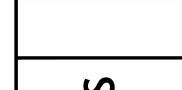
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ACK ROCK

NPE IMPROVEMEN

ENUE AND BREWSTER STREET

PPORT, CONNECTICUT

STREETSCAPE IMP FAIRFIELD AVENUE AND BE BRIDGEPORT, CON

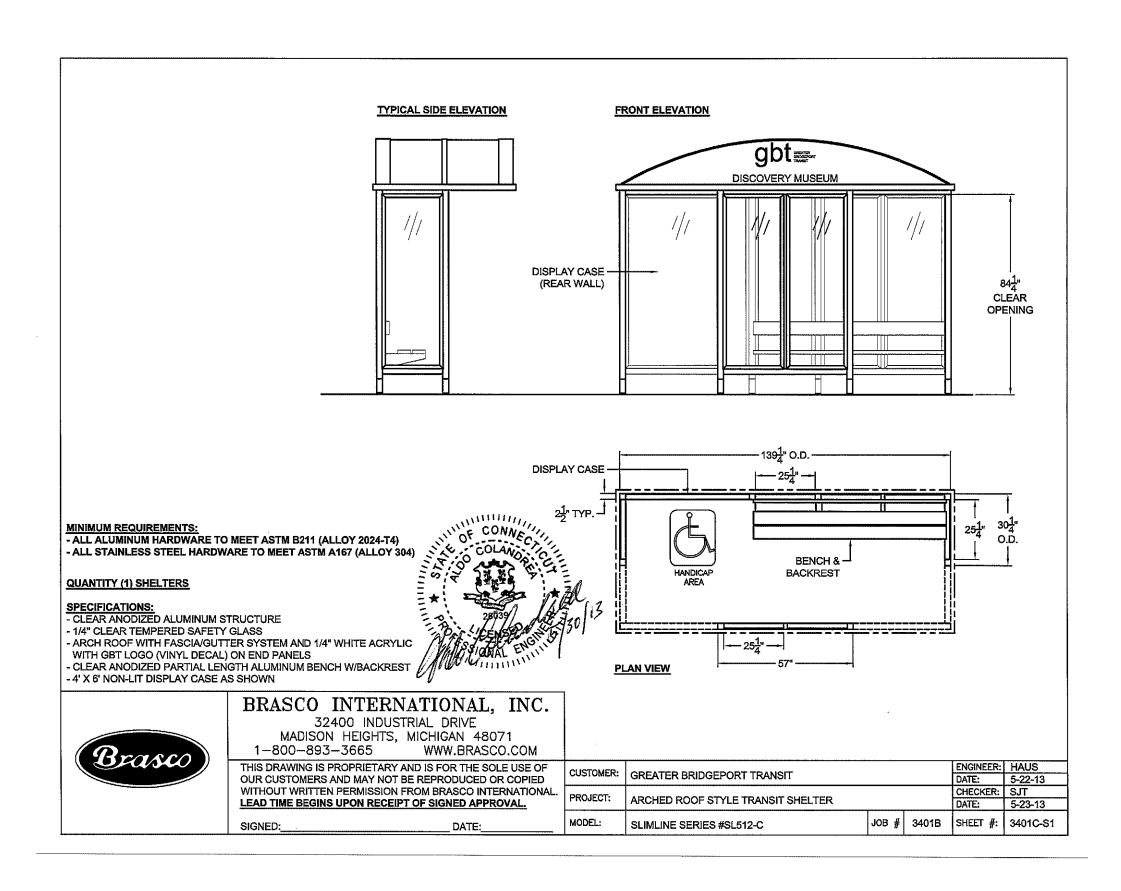
DESIGNED: S.W.
DRAFTED: K.M.
CHECKED: R.G.
APPROVED:
SCALE:
PROJECT NO.: 2016-

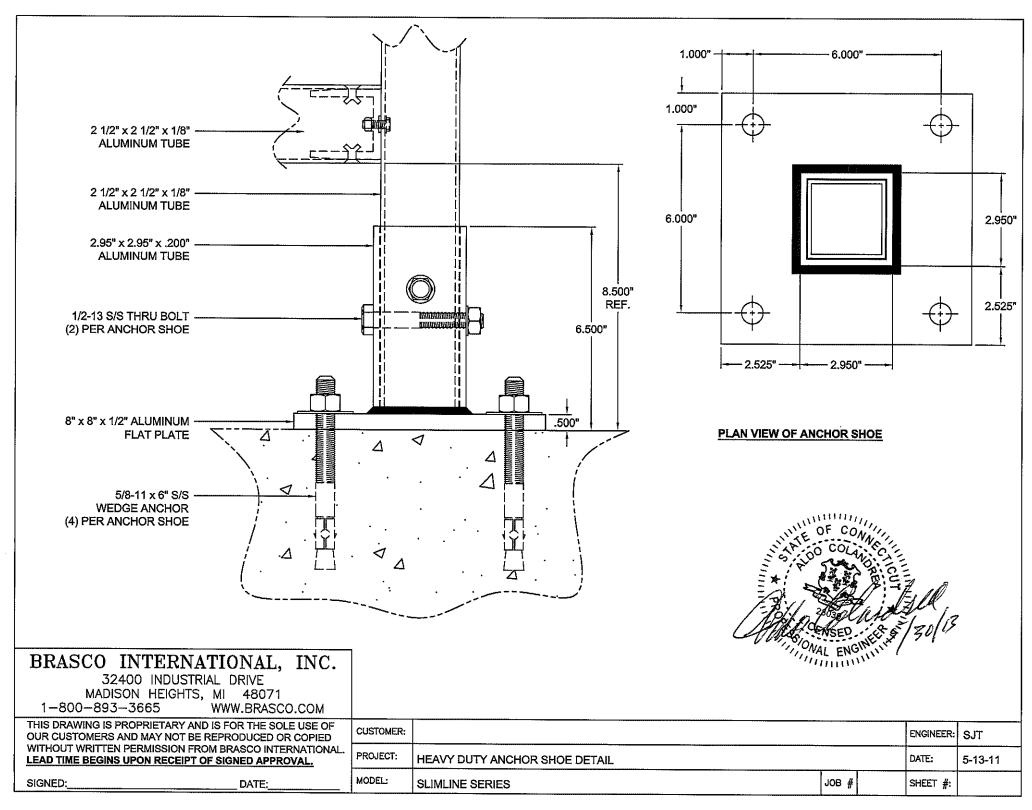
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DATE: 08/06/2018

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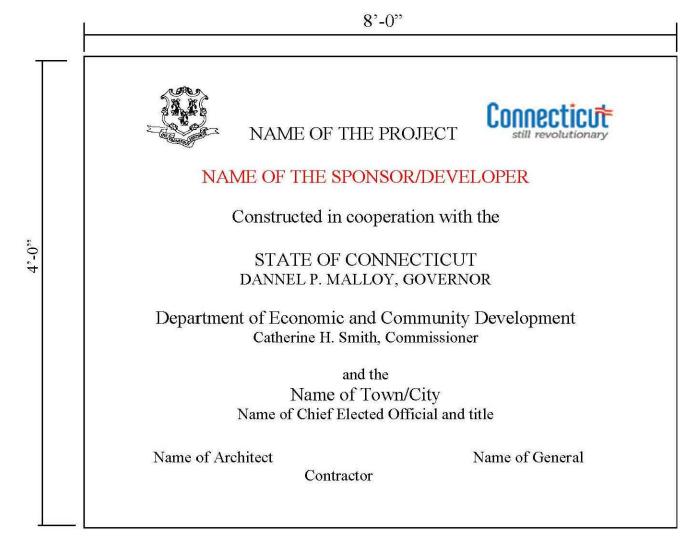
MISC. DETAILS





BUS SHELTER - N.I.C.

DEPARTMENT OF ECONOMIC & COMMUNITY DEVELOPMENT PROJECT SIGN



SIGN PANEL: 3/4" MDO-EXT-APA PLYWOOD SUPPORTED WITH (2) 4X4 TREATED WOOD COLUMNS AND SECURED 4' INTO GRADE. TOP OF SIGN AT 8'-0" ABOVE GRADE.

LORS: ALL LETTERS AND SYMBOLS ARE TO BE ROYAL BLUE. THE BACKGROUND WILL BE WHITE ENAMEL. BACK OF PLYWOOD AND SUPPORT STRUCTURE SHALL BE PAINTED MATTE BLACK.

TYPEFACE: HELVETICA MEDIUM

LOCATION: SIGN MUST BE LOCATED TO BE CLEARLY VISIBLE TO THE PUBLIC.

TIMING: INSTALL AT THE START OF CONSTRUCTION AND REMOVE AT CONSTRUCTION COMPLETION.

STATE SEAL & DECD LOGO: ATTACHED

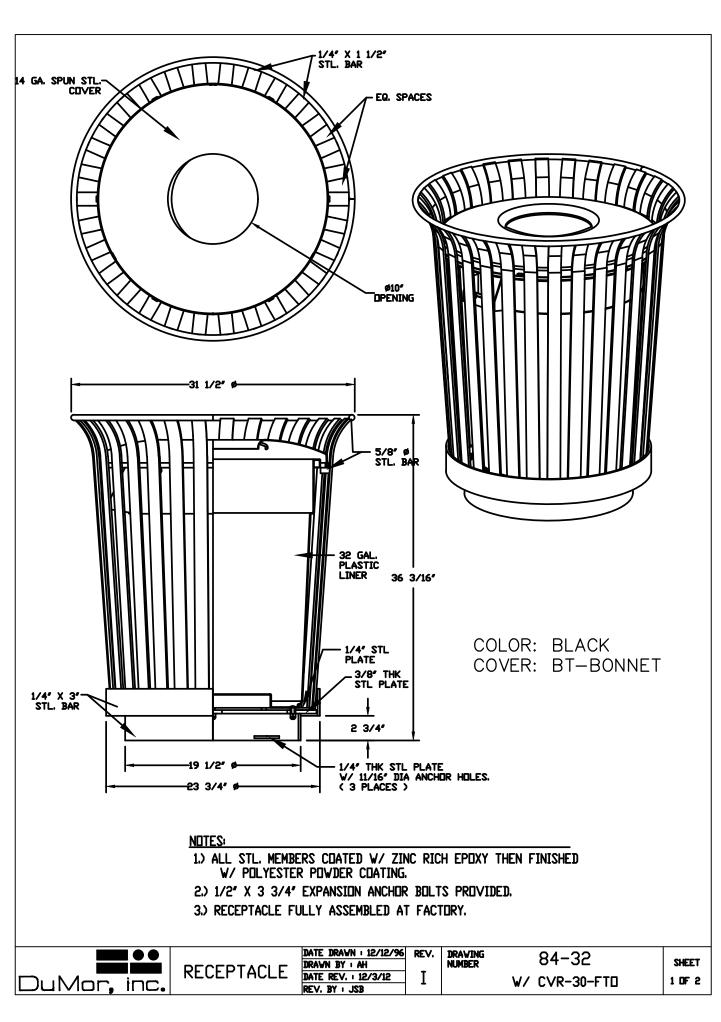


DECD LOGO

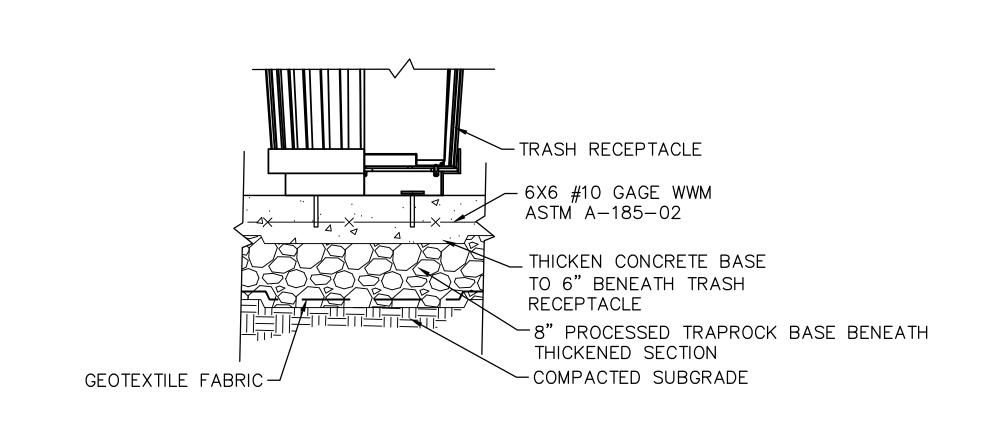


PROJECT CONSTRUCTION SIGN

N.T.S.







TRASH RECEPTACLE CONCRETE BASE

THE CITY OF BRIDGEPORT
999 BROAD STREET
BRIDGEPORT, CT

NO. DATE

NO. DATE

REVISIONS

FREEMAN

COMPANIES

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IMPROVEMENTS
AND BREWSTER STREET

BLACK ROCK
STREETSCAPE IMPRO
FAIRFIELD AVENUE AND BREWST

 DESIGNED:
 S.W.

 DRAFTED:
 K.M.

 CHECKED:
 R.G.

 APPROVED:
 SCALE:
 N.T.S.

 PROJECT NO.:
 2016-0238

 DATE:
 08/06/2018

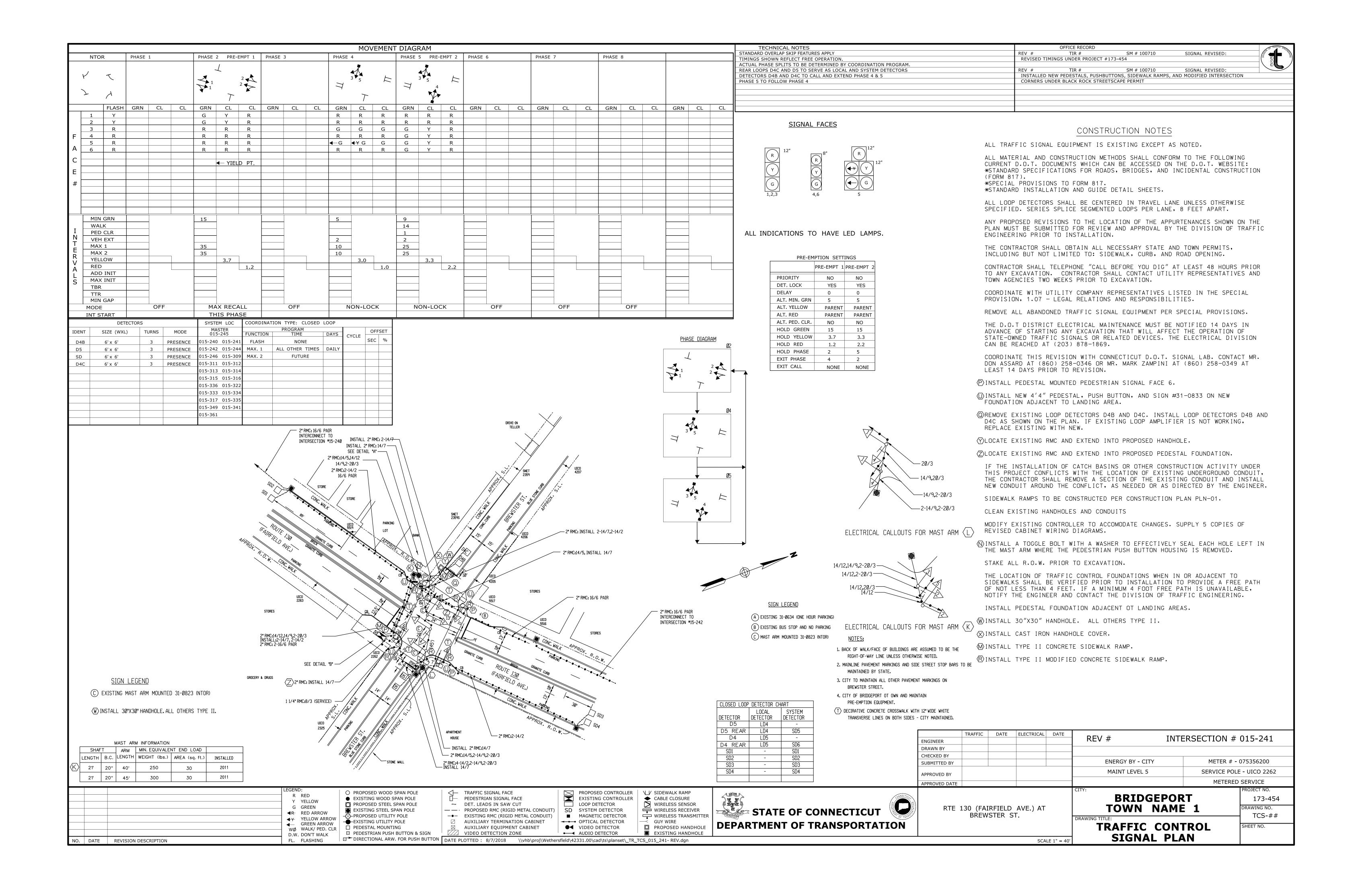
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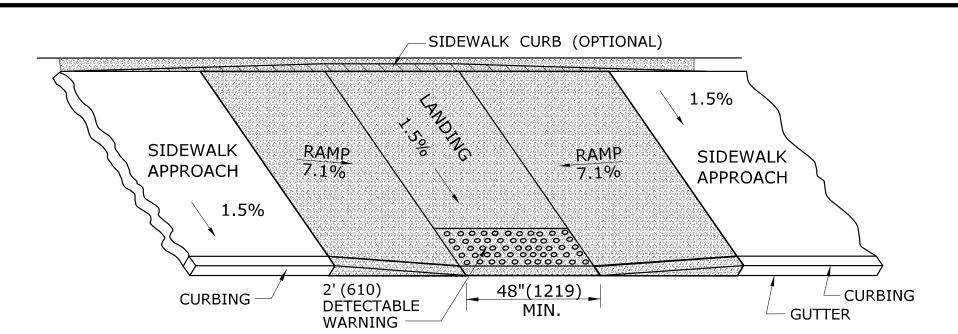
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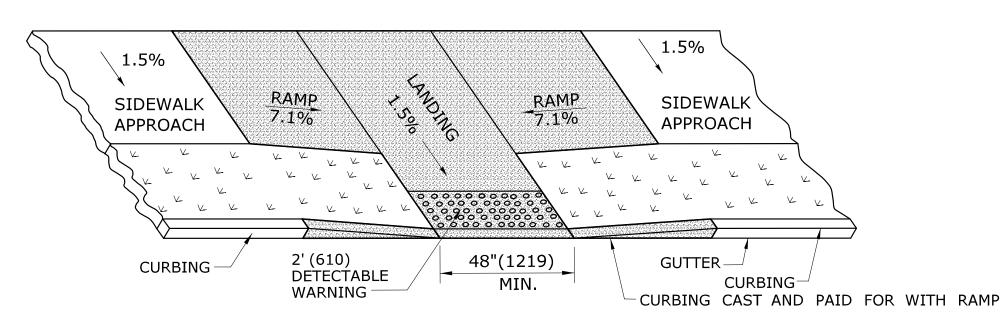
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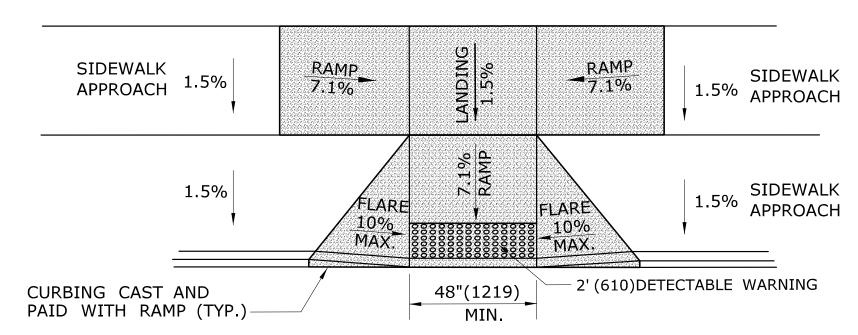




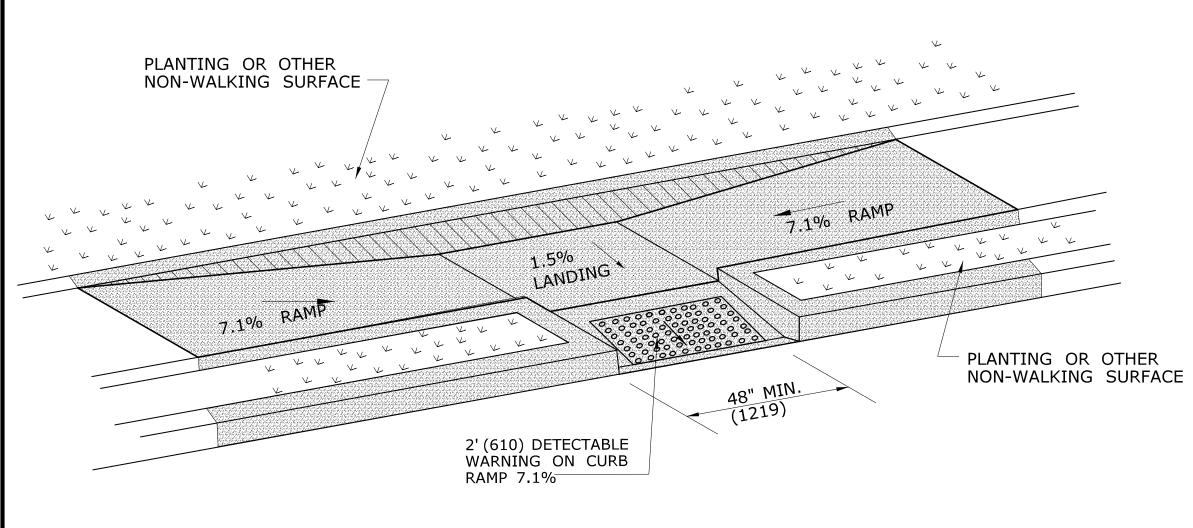
PARALLEL SIDEWALK RAMP (TYPE 1) NO UTILITY STRIP



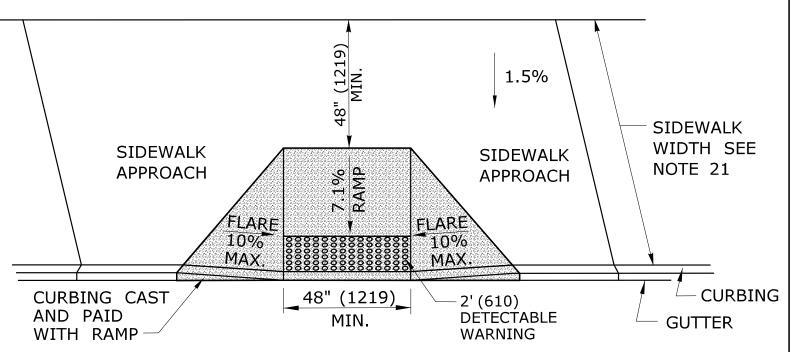
PARALLEL SIDEWALK RAMP (TYPE 1a) WITH UTILITY / GRASS STRIP



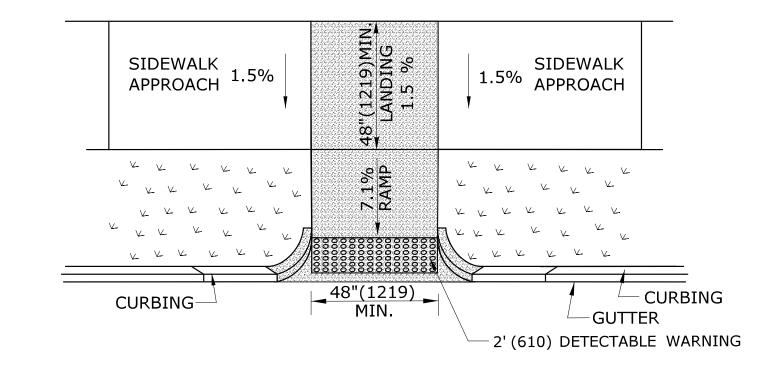
PARALLEL/PERPENDICULAR SIDEWALK RAMP NO UTILITY/GRASS STRIP (TYPE 1b)



PARALLEL SIDEWALK RAMP (TYPE 1c) WITH UTILITY / GRASS STRIP



PERPENDICULAR SIDEWALK RAMP W/ 48" (1219) MIN. BY PASS LANDING (TYPE 2)



PERPENDICULAR SIDEWALK RAMP W/CURB RETURNS / UTILITY GRASS STRIP (TYPE 2a)

* OPTIONAL FLARE ONE SIDE OF RAMP

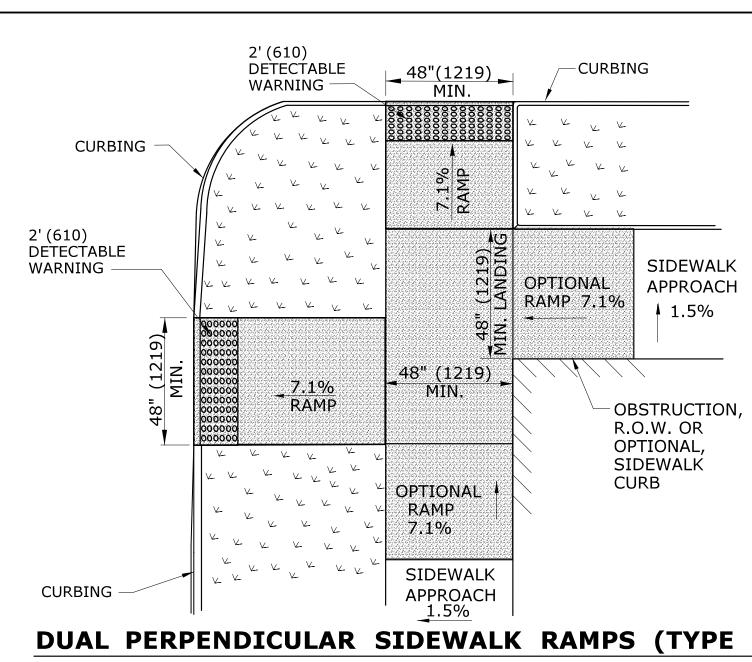
OPTIONAL RAMP 7.1% **CURBING** 2' (610) DETECTABLE WARNING-SIDEWALK CURB (OPTIONAL) RAMP OPTIO RAMP 7.1% CURBING CAST AND PAID WITH RAMP_ **CURBING** 48"(1219) CURBING CURBING CAST AND DETECTABLE PAID WITH RAMP-

DUAL PERPENDICULAR SIDEWALK RAMPS (TYPE 3)

SEE NOTES 19 * OPTIONAL CURB RETURN ON ONE SIDE OF RAMP ** SEE NOTE 23

GENERAL NOTES:

- 1. MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP SHOULD NOT EXCEED 5%. THE MAXIMUM GRADE DIFFERENCE BETWEEN THE GUTTER AND CURB RAMP SHALL NOT EXCEED 13%. SEE DETAIL 1 ON SHEET 4.
- 2. RAMP GRADE SHALL BE UNIFORM, FREE OF SAGS AND ABRUPT GRADE CHANGES. RUNNING SLOPES OF RAMPS SHALL NOT EXCEED 8.33% AND SHALL NOT EXCEED 15' (4.5m) WITHOUT PROVIDING A LANDING.
- 3. ALL RAMPS SHALL BE CONSTRUCTED OF CLASS "F" CONCRETE IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
- 4. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF THE RAMP. THE SURFACE OF ALL SIDEWALK RAMPS SHALL BE STABLE, FIRM AND SLIP RESISTANT. SURFACE DISCONTINUITIES SHALL NOT EXCEED $\frac{1}{2}$ " (13) MAX. VERTICAL DISCONTINUITIES BETWEEN $\frac{1}{4}$ " (6.4) AND $\frac{1}{2}$ " (13) MAX. SHALL BE BEVELED 1:2 MINIMUM APPLIED ACROSS THE ENTIRE LEVEL CHANGE
- 5. DIAGONAL SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE WHOLLY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES. DIAGONAL AND PERPENDICULAR RAMPS SHALL HAVE THE RAMP CUT PERPENDICULAR TO THE TANGENT OF THE CURB RADIUS FOR THE DESIGNATED ACCESSIBLE ROUTE. BOTH LONGITUDINAL SIDES OF THE RAMP SHOULD BE THE SAME LENGTH. SKEWED RAMPS SHOULD BE AVOIDED. FLARES ARE NOT CONSIDERED PART OF PEDESTRIAN ACCESS ROUTE. DIAGONAL RAMPS SHOULD NOT BE INSTALLED WHERE CURB RADII IS LESS THAN 20'(6096).
- 6. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT. 8.3% MAXIMUM SLOPE MAY NOT BE ACHIEVABLE DUE TO EXISTING SIDEWALK GRADE. IN RECOGNITION OF THIS, A LIMIT OF 15' (4572) FOR REMOVAL SHALL BE USED UNLESS OTHERWISE SHOWN ON THE PLANS OR DIRECTED BY THE ENGINEER. SAW CUT REQUIRED FOR DUMMY JOINTS SHALL BE INCLUDED IN THE COST OF
- "CONCRETE SIDEWALK RAMP" OR "CONCRETE SIDEWALK" 7. EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 12' (3658) UNLESS OTHERWISE NOTED.
- 8. CONCRETE SIDEWALK RAMPS, SHALL BE PAID FOR UNDER THE ITEM "CONCRETE SIDEWALK RAMP", AS DEFINED BY THE
- CONSTRUCTION LIMITS ON THE PLANS AND SHALL BE FIELD VERIFIED. 9. SIDEWALK RAMPS SHALL BE CONSTRUCTED WITH THE TOE AT THE GUTTER CAST INTEGRALLY WITH RAMP UNLESS DIRECTED OTHERWISE BY THE ENGINEER (SEE TYPICAL SECTION ON SHEET 3). CURB REMOVAL AND CAST IN PLACE CURBING
- REQUIRED FOR THE RAMP, SHALL BE INCLUDED WITH PAY ITEM "CONCRETE SIDEWALK RAMP" CURBING OUTSIDE LIMITS OF RAMP OR LANDING SHOWN ON SHEET 3 SHALL BE CONSTRUCTED AND PAID FOR IN ACCORDANCE WITH CONNECTICUT STANDARD SPECIFICATIONS.
- 0. PREFERRED LOCATION TO INSTALL DETECTABLE WARNING STRIP SHALL BE 6" (152) FROM THE EDGE OF ROAD ALONG THE FULL WIDTH OF THE RAMP FOR ALTERNATE LOCATIONS, REFER TO DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 4.
- 1. TO PERMIT WHEELCHAIR WHEELS TO ROLL BETWEEN DOMES, ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF RUNNING SLOPE (PERPENDICULAR TO CURB OR SLOPE BREAK). THE TRANSITION FROM RAMP TO GUTTER SHALL BE FLUSH WITHOUT A LIP.
- 12. WHERE COMMERCIAL DRIVEWAYS ARE PROVIDED WITH TRAFFIC SIGNALS AND THE SIDEWALK IS CONTINUOUS THROUGH DRIVEWAY, DETECTABLE WARNINGS ARE REQUIRED AT THE JUNCTION BETWEEN THE PEDESTRIAN ROUTE AND DRIVEWAY.
- 13. CONSTRUCT A SIDEWALK CURB WHEN THERE IS INSUFFICIENT BUFFER AVAILABLE TO GRADE OR WHEN CALLED FOR IN PLANS. PAID FOR WITH SIDEWALK RAMP WHEN REQUIRED FOR RAMP.
- 14. THE TOP AND BOTTOM OF RAMPS SHOULD BE PROVIDED WITH A 4'x 4' (1219 x 1219) MINIMUM LEVEL LANDING AREA WITH A CROSS SLOPE LESS THAN OR EQUAL TO 2% IN ANY DIRECTION 15. UTILITY POLES, LUMINAIRE, PEDESTRIAN OR SIGNAL POLES, GRATES, ACCESS COVERS, AND OTHER APPURTENANCES
- SHALL NOT BE LOCATED ON RAMPS, LANDINGS, BLENDED TRANSITIONS, AND @ GUTTERS WITHIN THE PEDESTRIAN ACCESS ROUTE.
- 16. APPROACH SIDEWALK WIDTHS, GRASS STRIP OR UTILITY STRIP WIDTHS MAY VARY. 17 APPROACH SIDEWALK AND LANDING CROSS SLOPE SHALL NOT EXCEED 2%
- 18. THE RUNNING OR CROSS SLOPES ON LANDINGS AT MID BLOCK CROSSING MAY BE WARPED TO MEET STREET OR
- HIGHWAY GRADE. 19. FOR PERPENDICULAR CURB RAMPS A MIN. $4'(1.2m) \times 4'(1.2m)$ LEVEL LANDING SHALL BE PROVIDED AT THE TOP OF CURB RAMP. WHERE THE LEVEL LANDING IS RESTRICTED AT THE BACK OF SIDEWALK THE LEVEL LANDING SHALL BE
- 4'(1.2m) x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE RAMP RUN. 20. FOR PARALLEL CURB RAMPS, A MIN. 4'(1.2m) imes 4'(1.2m) LEVEL LANDING SHALL BE PROVIDED AT THE BOTTOM OF CURB
- RAMP. IF THE LEVEL LANDING IS RESTRICTED ON 2 OR MORE SIDES, THE LEVEL LANDING SHALL BE 4'(1.2m)x 5'(1.5m) WITH THE 5'(1.5m) DIMENSION PROVIDED IN THE DIRECTION OF THE PEDESTRIAN STREET CROSSING. 21. WHEN WIDTH OF SIDEWALK IS >48" AND A PERPENDICULAR SIDEWALK RAMP IS INSTALLED, THE FLARED SIDES SHALL
- BE 10% MAX. IF WIDTH OF SIDEWALK IS <48" THE FLARED SIDES MUST NOT EXCEED 8.33% (12:1). 22. SHADED AREAS ARE TYPICAL PAY LIMITS FOR CONCRETE SIDEWALK RAMP BUT, MAY VARY AS DIRECTÉD BY THE ENGINEER



DUAL PERPENDICULAR SIDEWALK RAMPS (TYPE 3a) WITH UTILITY / GRASS STRIP

SEE NOTE 20

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

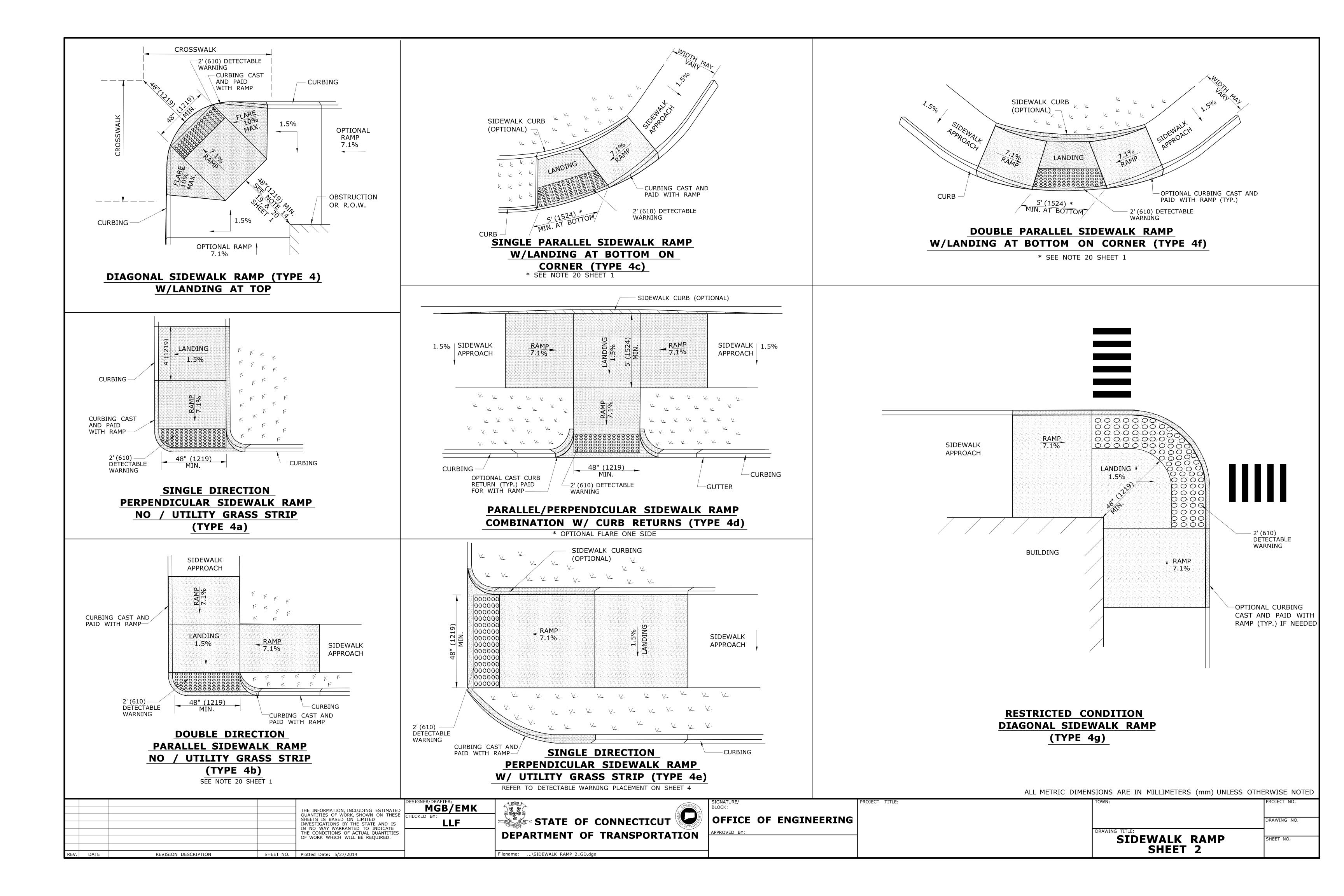
STATE OF CONNECTICUT SIGNATUR BLOCK: MGB/EMK THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. OFFICE OF ENGINEERING LLF - | -**DEPARTMENT OF TRANSPORTATION** 7/13 Created new sheets (4 total). REVISION DESCRIPTION SHEET NO. Plotted Date: 6/17/2014 Fllename: ...\SIDEWALK RAMP 1_GD.dgn REV. DATE

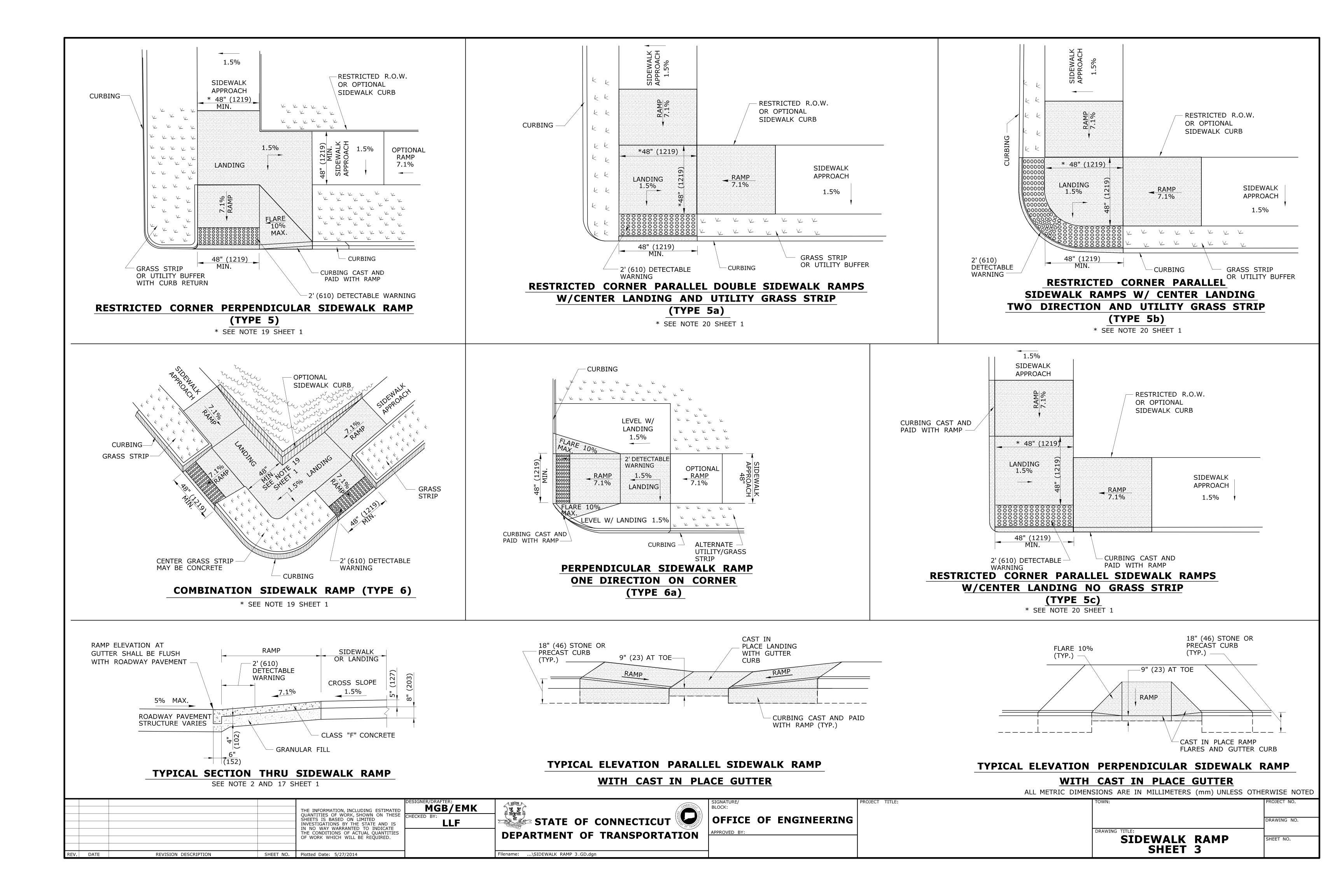
SIDEWALK RAMPS

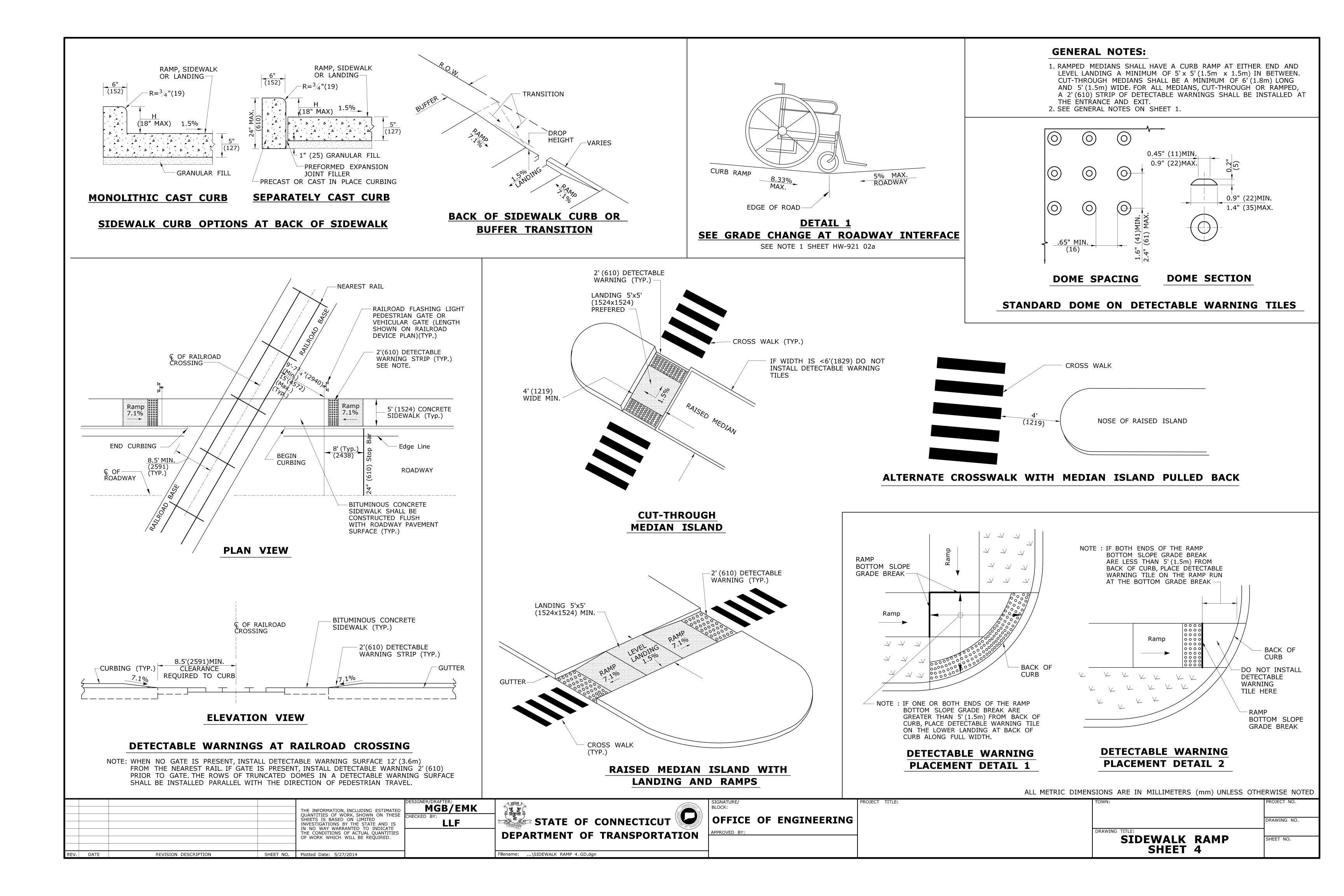
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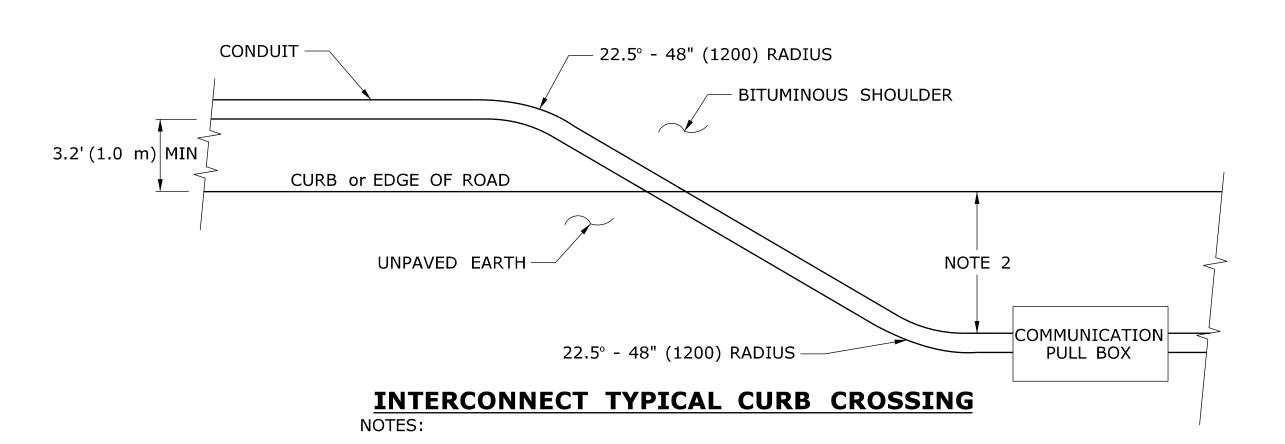
SHEET NO.

SHEET 1



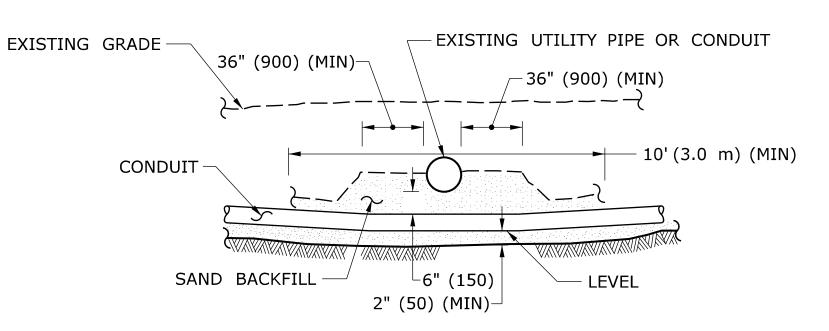






1. RESTORE AREAS DISTURBED BY TRENCH TO ORIGINAL CONDITION.

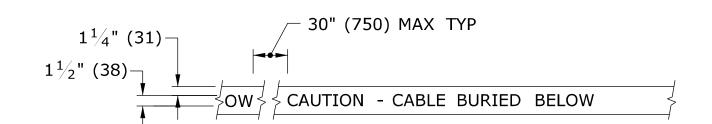
2. INSTALL PULL BOX A MINIMUM OF 10' (3.0 m) FROM CURB UNLESS OTHERWISE SHOWN ON PLANS OR DIRECTED BY ENGINEER.



CROSSING UNDER EXISTING UTILITY

NOTES:

- 1. WHEN ENCOUNTERED AT APPROXIMATELY THE SAME DEPTH, CROSS BENEATH.
- 2. PROTECT & SUPPORT EXPOSED EXISTING UTILITY.

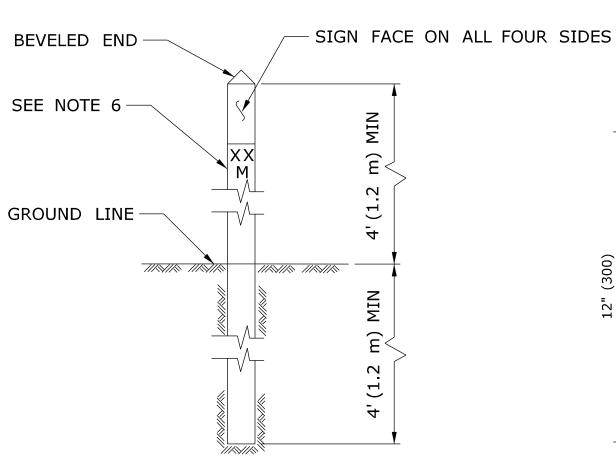


DETECTABLE WARNING TAPE

NOTE:

STANDARD SPECIFICATIONS, ARTICLE: 1.05.15

1. TAPE COLORS: COMMUNICATION - ORANGE BACKGROUND / BLACK LEGEND POWER - RED BACKGROUND / BLACK LEGEND



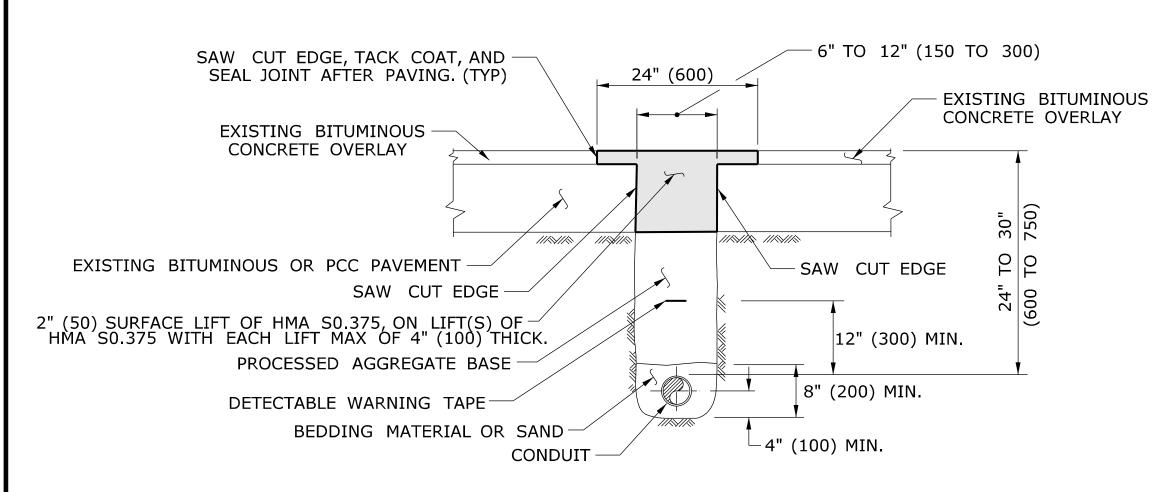
$--2^{3}/_{4}$ " (69) (TYP) $\sqrt{3/8}$ " (9) (TYP) $\frac{1}{2}$ " (13) (TYP) $\mathsf{COMMUNICATION}^\dagger$ CONDUIT BURIED BELOW CALL BEFORE YOU DIG 1 - 800 922 - 4455 SIGN FACE DETAIL

INTERCONNECT CONDUIT IDENTIFICATION POST

SIGN # 41-4669

NOTES:

- 1. 4" x 4" (100 x 100) NOMINAL, PRESSURE TREATED WOOD POST.
- 2. ATTACH SIGN TO POST WITH $\frac{1}{4}$ " x $1\frac{1}{4}$ " (6 x 31) STAINLESS STEEL LAG SCREW WITH NYLON WASHER ON FACE OF SIGN.
- 3. SIGN COLORS: BACKGROUND ORANGE (RETROREFLECTIVE) LEGEND - BLACK (OPAQUE).
- 4. INSTALL POST APPROX 24" (600) FROM RMC IN VICINITY OF EACH PULL BOX.
- 5. INSTALL POSTS BETWEEN PULL BOXES, APPROX 10' (3.0 m) OFF CURB. SPACE POSTS 1500'± (460 m±) APART.
- 6. PERMANENTLY ATTACH STAINLESS STEEL NUMBERS INDICATING DISTANCE TO TRENCH IN FEET (METERS) CONTAINING COMMUNICATION CABLE. ATTACH NUMBERS TO SIDE OF POST FACING CONDUIT. INCLUDE "M" SUFFIX IF METERS.

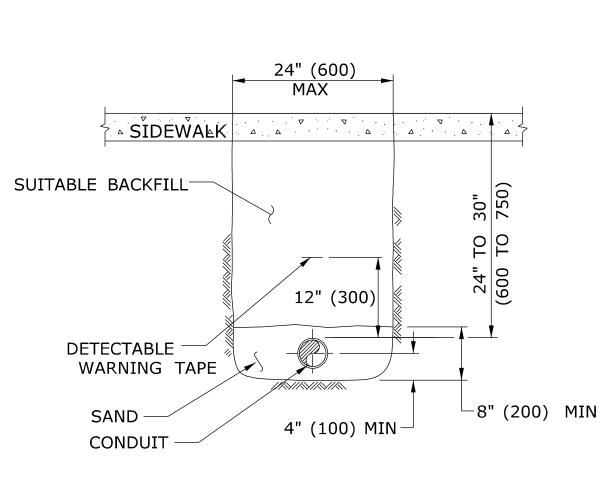


PAVEMENT - BITUMINOUS CONCRETE OR OVERLAYED PORTLAND CEMENT CONCRETE

NOTES:

STANDARD SPECIFICATIONS, ARTICLE: 3.04 & 4.06.03

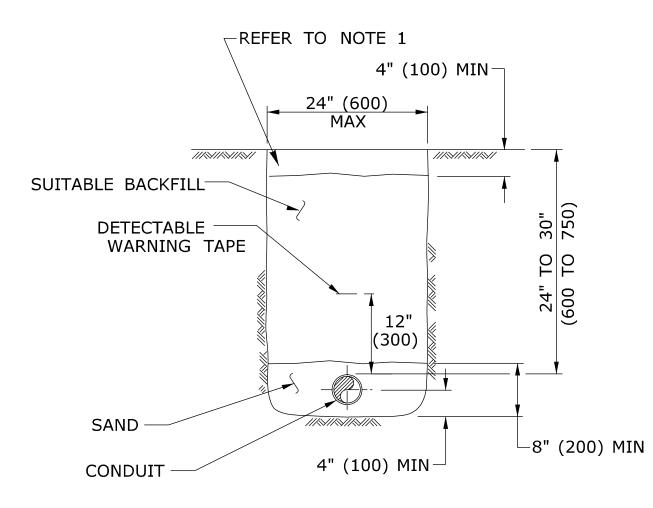
- 1. TOTAL HOT MIX ASPHALT (HMA) THICKNESS TO MATCH EXISTING BITUMINOUS CONCRETE AND PORTLAND CEMENT CONCRETE (PCC) THICKNESS.
- 2. WHEN ALLOWED BY ENGINEER, USE CONTROLLED LOW STRENGTH MATERIAL (CLSM) AS BEDDING MATERIAL. TOP OF CLSM AT LEAST 20" (500) BELOW SURFACE.



SIDEWALK

NOTES: STANDARD SPECIFICATIONS, ARTICLE: 9.21 & 9.22

1. WHERE CONCRETE SIDEWALK DAMAGED OR CUT, REPLACE THE ENTIRE SECTION BETWEEN JOINTS. REPLACEMENT SIDEWALK IS PAID FOR AT THE CONTRACT UNIT PRICE FOR "CONCRETE SIDEWALK".



GENERAL NOTES:

- 1. TOP OF CONDUIT NO LESS THAN 24" (600) DEEP.
- 2. COMPACT BACKFILL IN ≤6" (150) LIFTS. HAND COMPACTION NOT PERMITTED.

EARTH

NOTES:

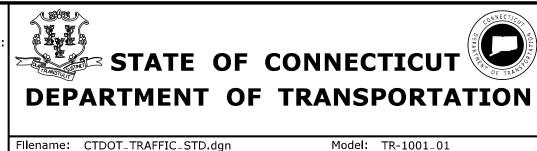
STANDARD SPECIFICATIONS, ARTICLE: 9.50

1. IN MOWED AREAS: PLACE TOPSOIL, FERTILIZER, SEED, & MULCH.

EGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN: -- RMC (RIGID METAL CONDUIT)

1	4-2012	REVISED BITUMINOUS CONRCETE TO HMA, & MINOR REVISIONS.	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.
1	4-2012	REVISED BITUMINOUS CONRCETE TO HMA, & MINOR REVISIONS.	
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 4/14/2012

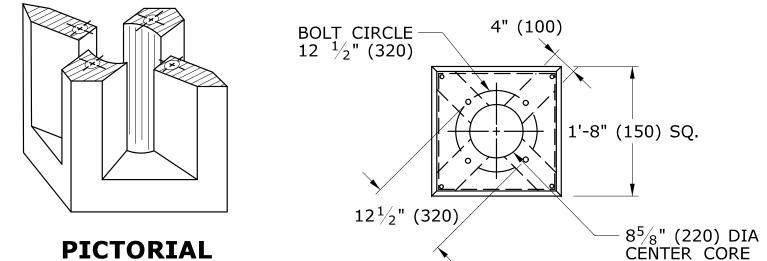
DIMENSIONS ARE IN ENGLISH ('.") & METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm - UNDER 1" TO NEAREST 1 mm. NOT TO SCALE



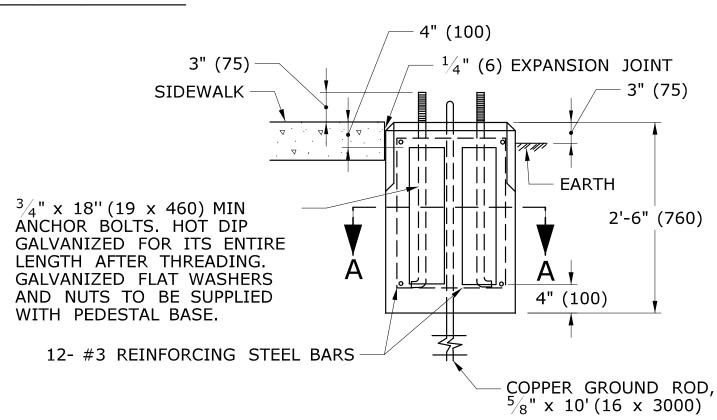
	SUBMITTED BY:	NAME/DATE/TIME:		
174T10N	Tracy L. Fogarty 2012.05.01 12:54:42-04'00'		CTDOT STANDARD SHEET	
ĺ	APPROVED BY:	NAME/DATE/TIME:		
•	TUIS	Timothy M. Wilson 2012.05.09 10:23:34-04'00'	OFFICE OF ENGINEERING	

TRENCHING & BACKFILLING, **ELECTRICAL CONDUIT**

TR-1001_01



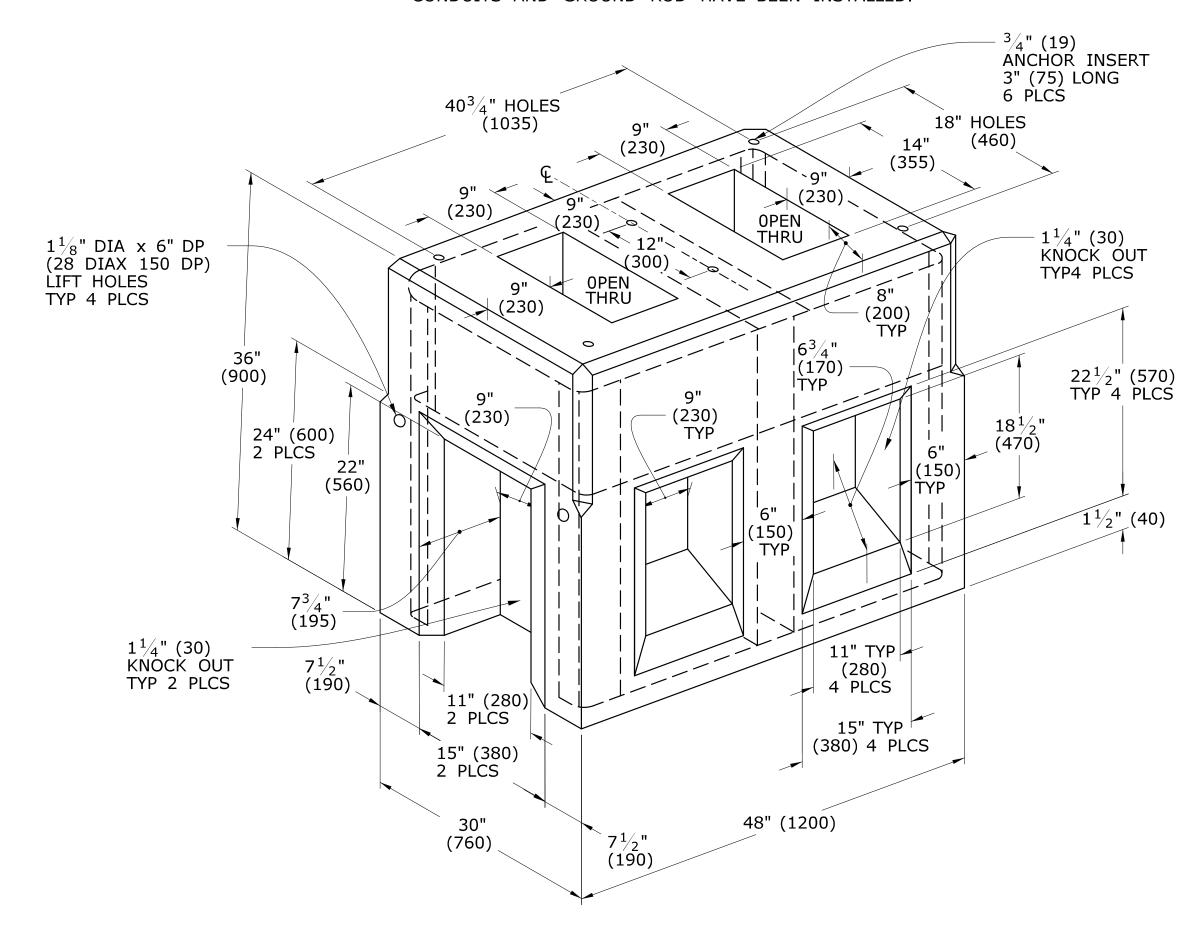
PICTORIAL SECTION A-A



TRAFFIC CONTROL FOUNDATION PEDESTAL - TYPE I - PRECAST

NOTES:

PLACE NO. 6 CRUSHED STONE IN CENTER OPENING AFTER CONDUITS AND GROUND ROD HAVE BEEN INSTALLED.



EGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN: PROPOSED CONTROLLER EXISTING CONTROLLER PROPOSED STEEL SPAN POLE

REVISION DESCRIPTION

EXISTING STEEL SPAN POLE

REV. DATE

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. 1-2014 REMOVED SPAN POLE FOUNDATION DETAILS, REVISED TYPICAL CONCRETE SIDEWALK AT CONTROLLER FOUNDATION. 1 4-2012 MINOR REVISIONS.

Plotted Date: 1/7/2014

DIMENSIONS ARE IN ENGLISH ('.") & METRIC UNITS (mm). ETRIC DIMENSIONS ARE ROUNDED: OVER 1" TO NEAREST 5 mm UNDER 1" TO NEAREST 1 mm. NOT TO SCALE

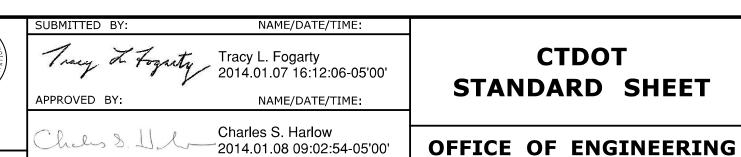
TRAFFIC CONTROL FOUNDATION

CONTROLLER - TYPE IV - PRECAST

STATE OF CONNECTICUT **DEPARTMENT OF TRANSPORTATION**

Model: TR-1002_01

Filename: CTDOT_TRAFFIC_STD.DGN



CTDOT STANDARD SHEET

TR-1002_01

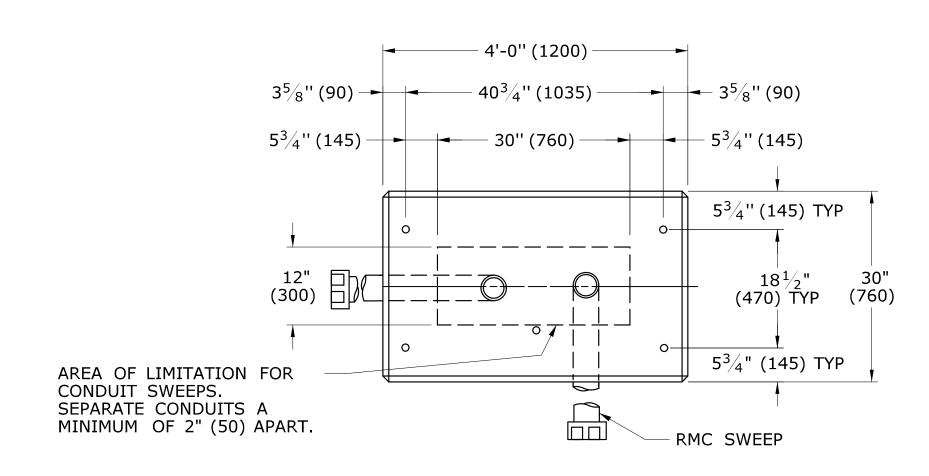
CONTROLLER FOUNDATION 3' (900)

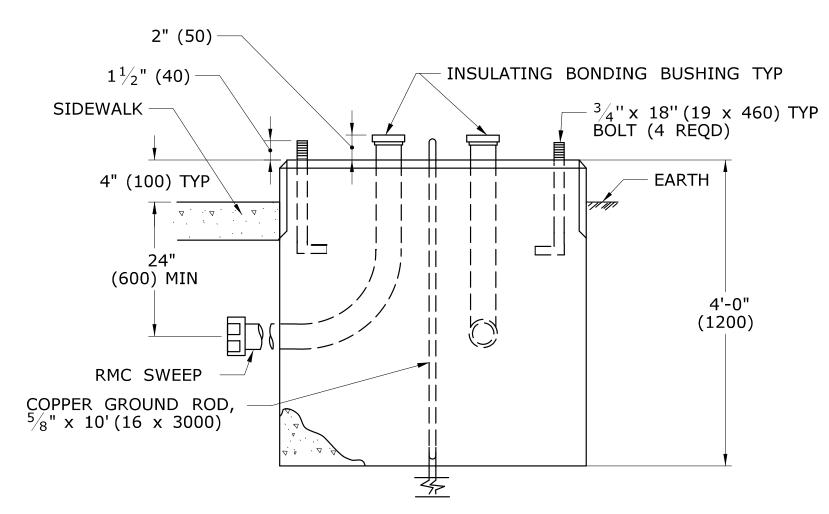
INSTALL PRECAST OR CAST IN PLACE CONCRETE SIDEWALK ON CABINET DOOR SIDE OF CONTROLLER FOUNDATION.

PITCH SIDEWALK $\frac{1}{4}$ " PER FOOT (20 PER METER) AWAY FROM THE CONTROLLER FOUNDATION.

REFER TO HIGHWAY STANDARD SHEET HW-921_01 FOR SIDEWALK CONSTRUCTION.

TYPICAL CONCRETE SIDEWALK AT CONTROLLER FOUNDATION





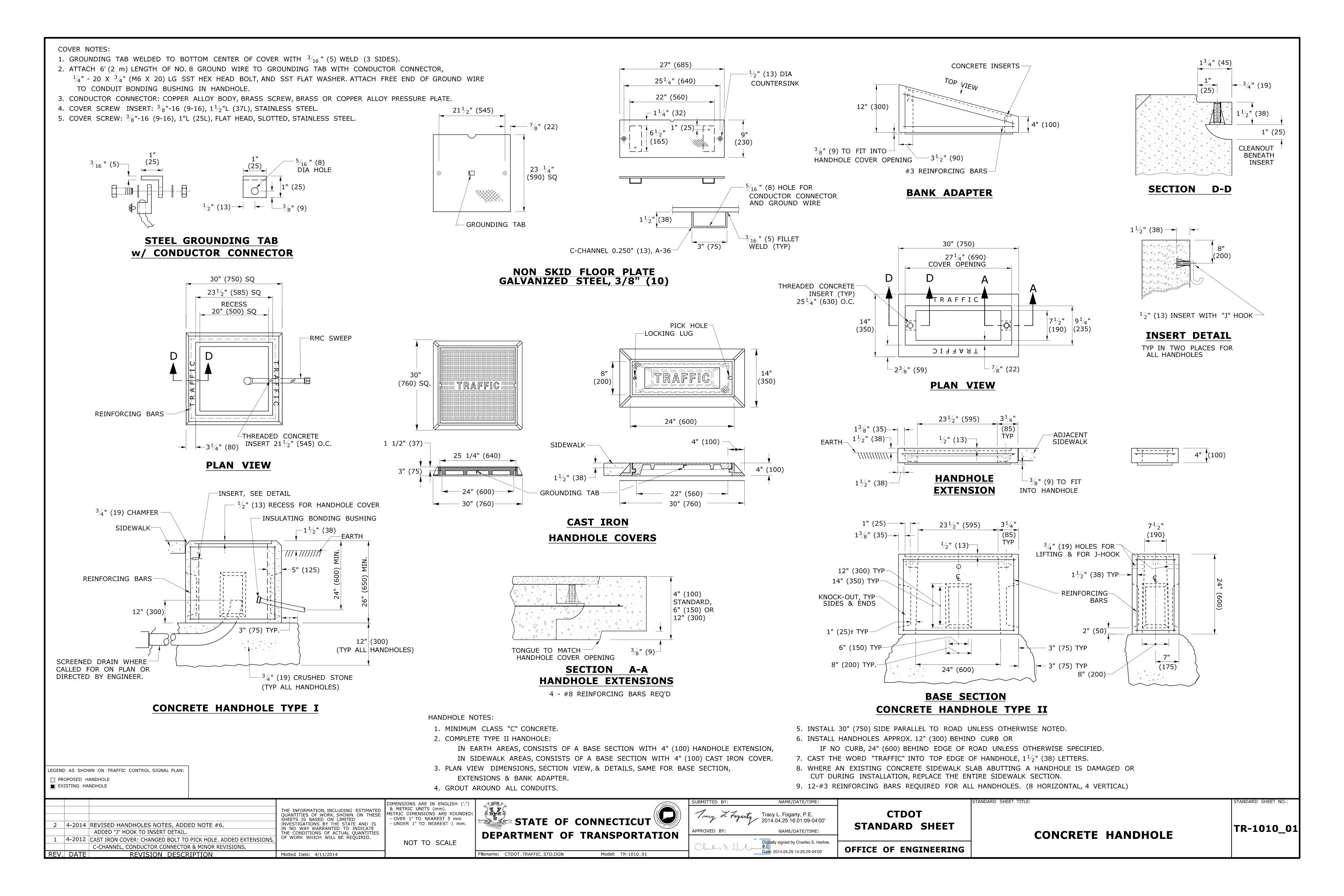
TRAFFIC CONTROL FOUNDATION **CONTROLLER - TYPE IV - CAST IN PLACE**

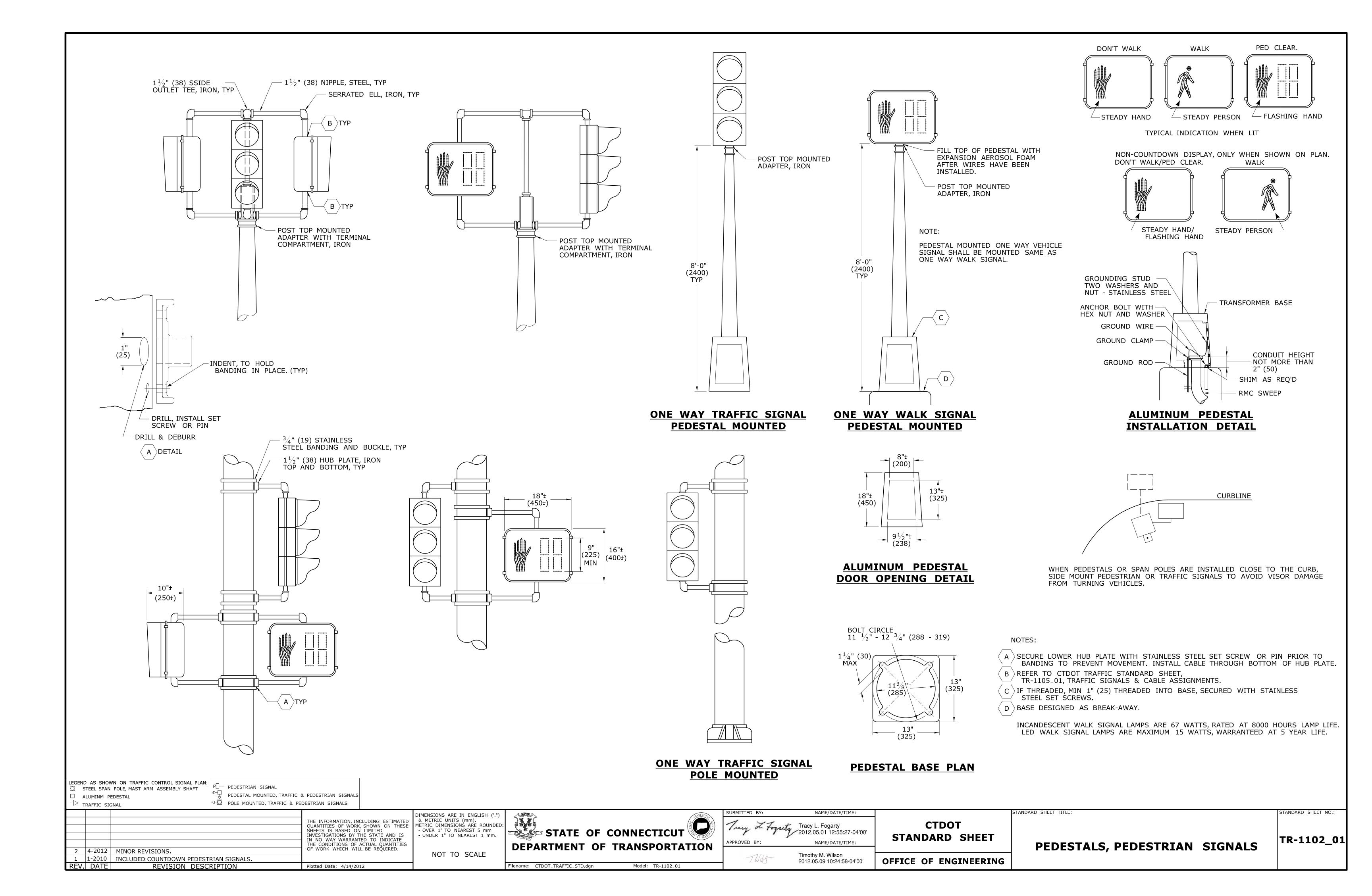
NOTES:

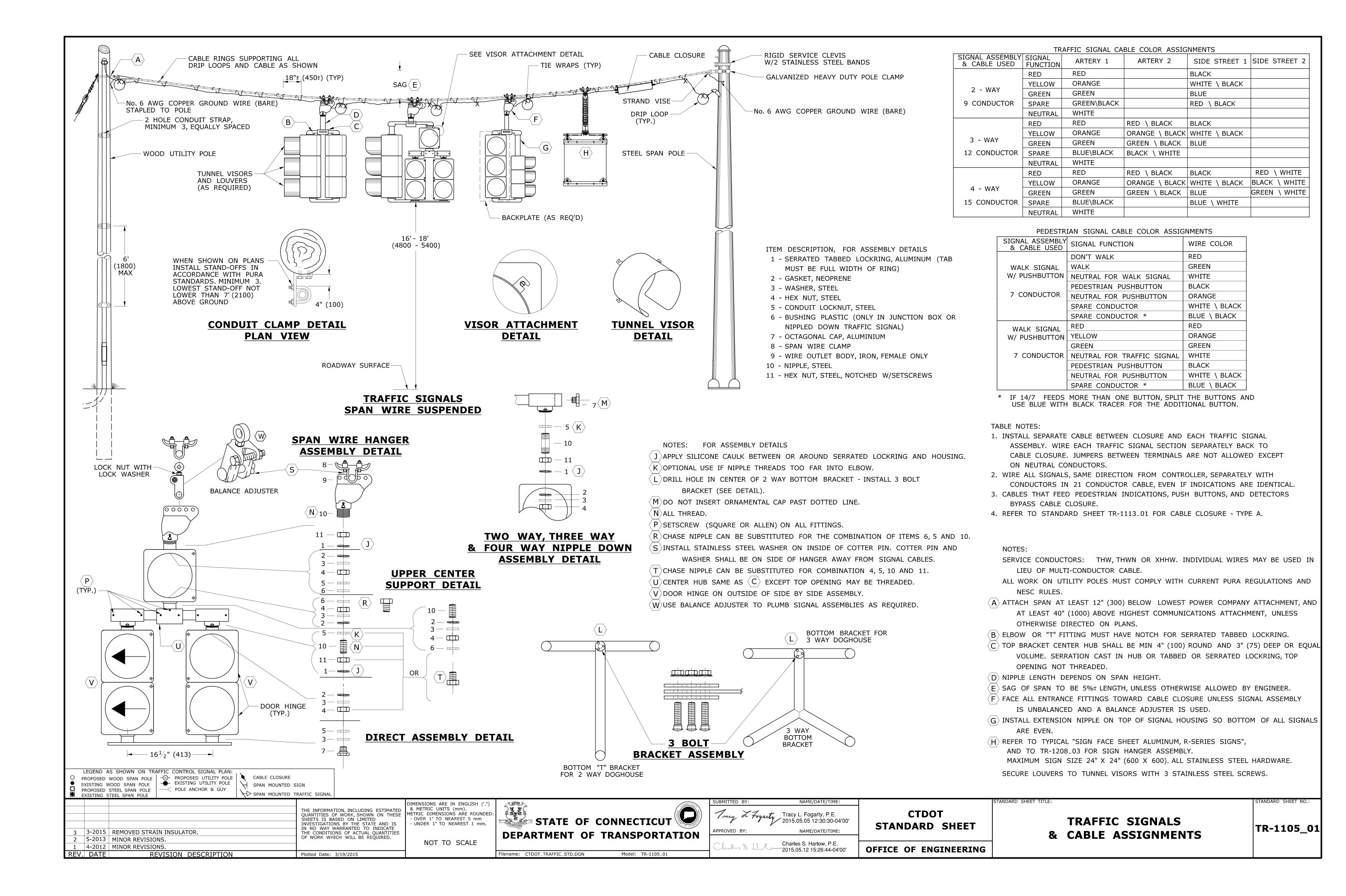
INSTALL FOUNDATION ON 6" (150) OF COMPACTED GRAVEL IN ACCORDANCE WITH SECTION 2.14. LEVEL FOUNDATION WITH A PROJECTION OF 4" (100) ABOVE FINISHED GRADE. INSTALL COPPER GROUND ROD: $\frac{5}{8}$ " x 10 (16 x 3000). PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND GROUND ROD HAVE BEEN INSTALLED. THE OPENINGS SHALL BE CAPPED WITH A 2" (50) GROUT LEVEL WITH THE TOP OF THE FOUNDATION AND NEATLY FINISHED. THE GROUT SHALL CONFORM WITH THE REQUIREMENTS OF ARTICLE M.3.01-12. CONCRETE: CLASS "A" CONFORMING TO ARTICLE M.03.01.

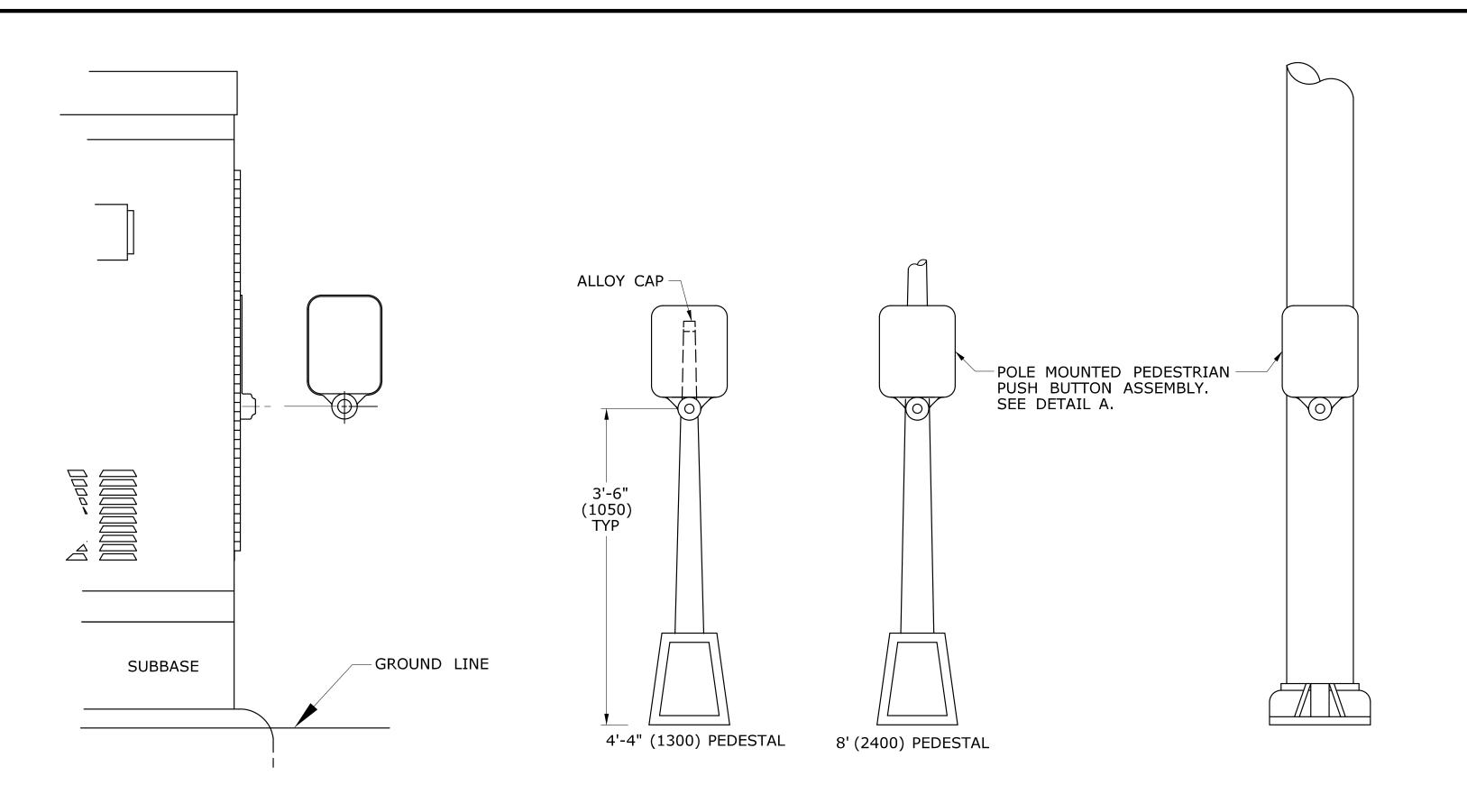
#4 REBAR 2" (50) MIN COVER AROUND ALL OPENINGS, 3-#4 REBARS IN EACH CORNER. CONDUITS SHALL NOT PROJECT MORE THAN 2" (50) ABOVE FOUNDATION.

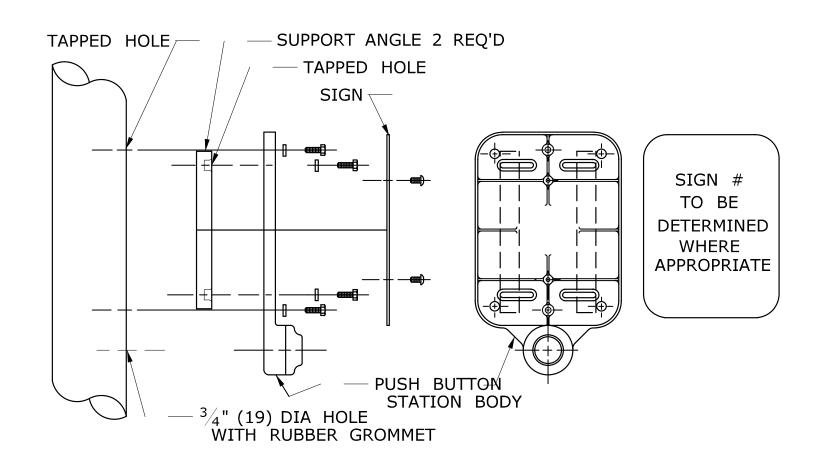
TRAFFIC CONTROL FOUNDATIONS











DETAIL A

PUSH BUTTON FOR GREEN LIGHT

SIGN # 31-0833

** USE APPROPRIATE ARROW UNLESS OTHERWISE NOTED ON PLAN.

PUSH BUTTON FOR GREEN LIGHT

SIGN # 31-0835

FOR CROSSING
WITH SIDE STREET GREEN

SURFACE MOUNTED

PEDESTAL MOUNTED

SPAN POLE/MAST ARM MOUNTED

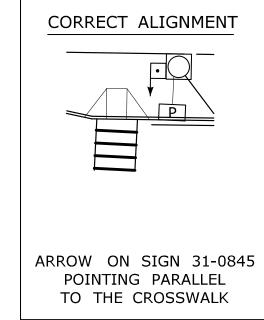
GENERAL NOTES:

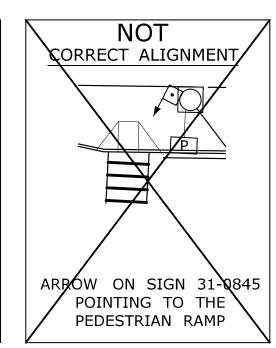
3'-6" (1050) FROM FINISHED GRADE SUCH AS SIDEWALK TO CENTER OF PUSH BUTTON.

PUSH BUTTON INSTALLATIONS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS

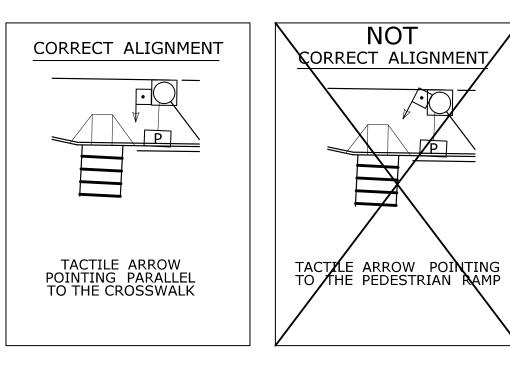
WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN, CURRENT EDITION GOVERNS.

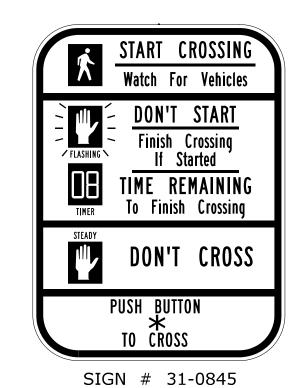
4'-4" (1300) PEDESTAL TO INCLUDE ALLOY CAP SECURED WITH STAINLESS STEEL SET SCREW.





PEDESTRIAN PUSH BUTTON ALIGNMENT





* USE APPROPRIATE ARROW UNLESS OTHERWISE NOTED ON PLAN.

ACCESSIBLE PEDESTRIAN SIGNAL AND DETECTOR

EXAMPLE ALIGNMENTS
FOR EXCLUSIVE PEDESTRIAN PHASE

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

PEDESTRIAN PUSH BUTTON

PEDESTRIAN POSH BUTTON, PEDESTAL MOUNTED

PEDESTRIAN PUSH BUTTON, POLE MOUNTED

REV. DATE

		'	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE
			IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES
2	4-2014	ADDED PEDESTRIAN EXAMPLE ALIGNMENTS	OF WORK WHICH WILL BE REQUIRED.
1	4-2012	MINOR REVISIONS & UPDATED SIGN #31-0845	

Plotted Date: 4/25/2014

REVISION DESCRIPTION

DIMENSIONS ARE IN ENGLISH ('.")

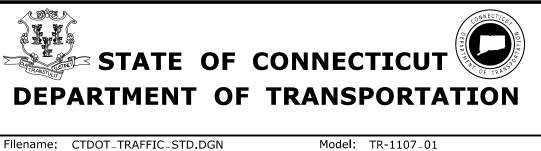
& METRIC UNITS (mm).

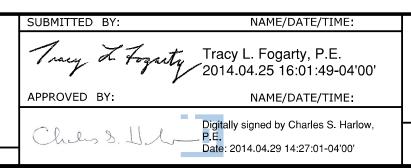
METRIC DIMENSIONS ARE ROUNDED:

OVER 1" TO NEAREST 5 mm

UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE



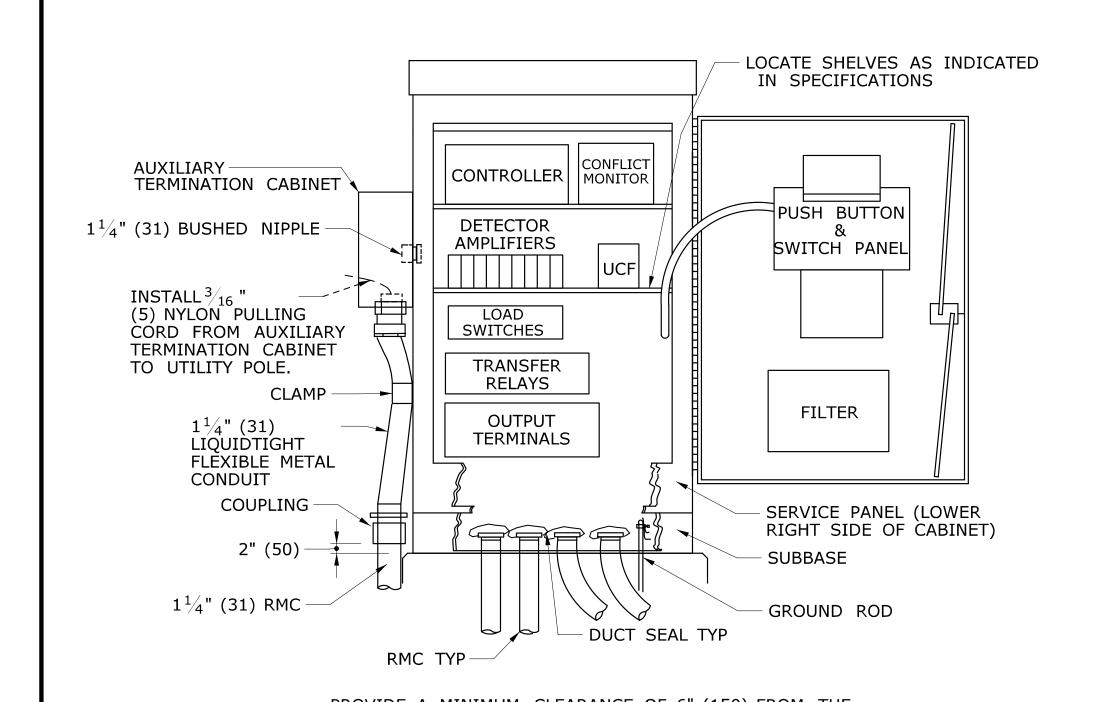


CTDOT
STANDARD SHEET

OFFICE OF ENGINEERING

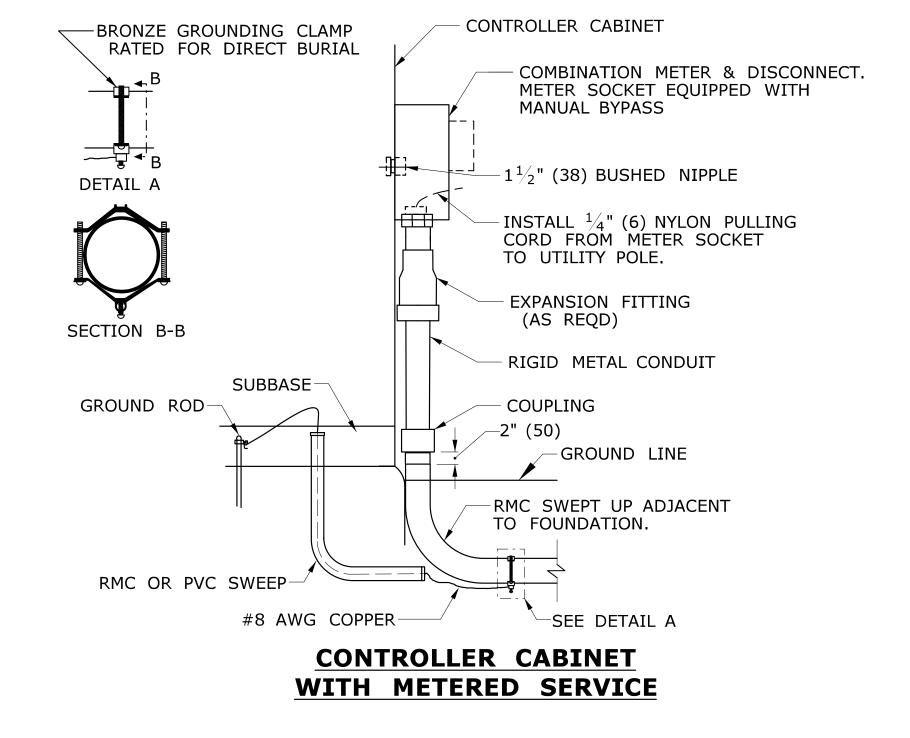
PEDESTRIAN PUSH BUTTONS

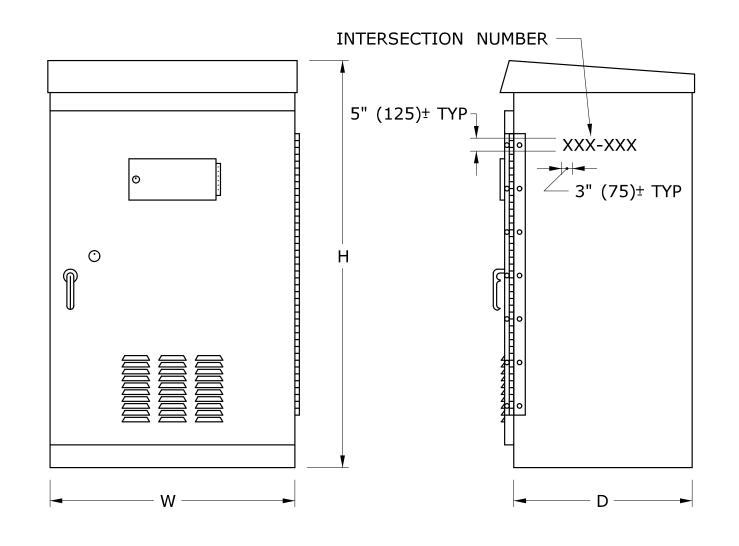
TR-1107_01



PROVIDE A MINIMUM CLEARANCE OF 6" (150) FROM THE CABINET BASE TO ALL COMPONENTS AND TERMINALS.

TYPICAL BASE MOUNTED CONTROLLER ON TYPE IV FOUNDATION





BASE MOUNTED TRAFFIC CONTROLLER (TYPE B, D & E)

			_			
CABINET	DEPTH		WIDTH		HEIGHT	
TYPE	MIN	MAX	MIN	MAX	MIN	MAX
В	17" (425)	19" (475)	30" (750)	34" (850)	52" (1300)	56" (1400)
D	25" (625)	27" (675)	42" (1050)	45" (1125)	54" (1350)	59" (1475)
Е	17" (425)	19" (475)	30" (750)	32" (800)	49" (1225)	52" (1300)

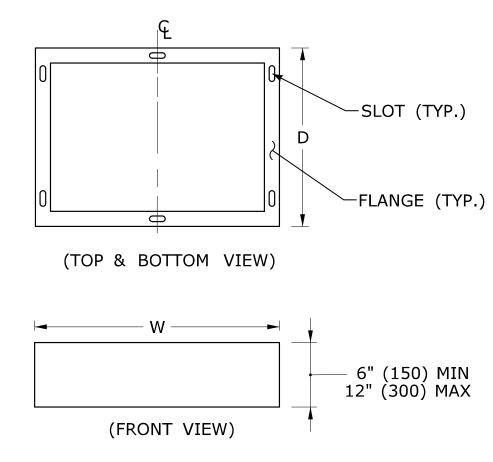
AUXILIARY EQUIPMENT CABINET (AEC) AUXILIARY TERMINATION CABINET (ATC)

O AEC

ATC

(1200)

CABINET TYPE	HEIGHT	WIDTH	DEPTH
ATC	16"(400)	12"(300)	6"(150)
AEC	14"(350)	11"(275)	11"(275)



SUBBASE

SLOT AND FLANGE DIMENSIONS TO BE PER MANUFACTURER.

GENERAL NOTES:

GROUT ALL BASES AFTER MOUNTING ON FOUNDATIONS, WHERE NECESSARY.

3'-0" (900) FROM SIDEWALK TO BOTTOM OF CONTROLLER.

INSTALL PEDESTALS AND POLES SO THAT DOORS AND COVERS ARE ON THE SIDE AWAY FROM THE STREET, UNLESS OTHERWISE SPECIFIED.

INSTALL CABINET SO THAT DOOR OPENS FIELD SIDE UNLESS OTHERWISE NOTED ON PLANS. CAULK SEAM BETWEEN SUBBASE AND FOUNDATION.

STENCIL SIX DIGIT INTERSECTION NUMBER, USING BLACK PAINT ON SIDE, FRONT OR BACK OF CABINET MOST VISIBLE FROM THE ROAD.

Plotted Date: 5/15/2013

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

CONTROLLER ASSEMBLY

AUXILIARY EQUIPMENT CABINET

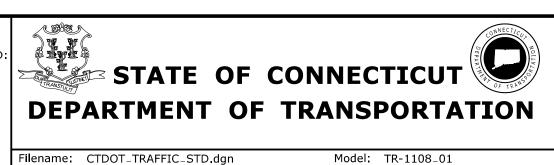
REV. DATE

	AUXILIAR	TERMINTION CABINET		
2	-	REVISED SUBBASE. REVISED CABINET TYPES & MINOR REVISIONS.	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	M

REVISION DESCRIPTION

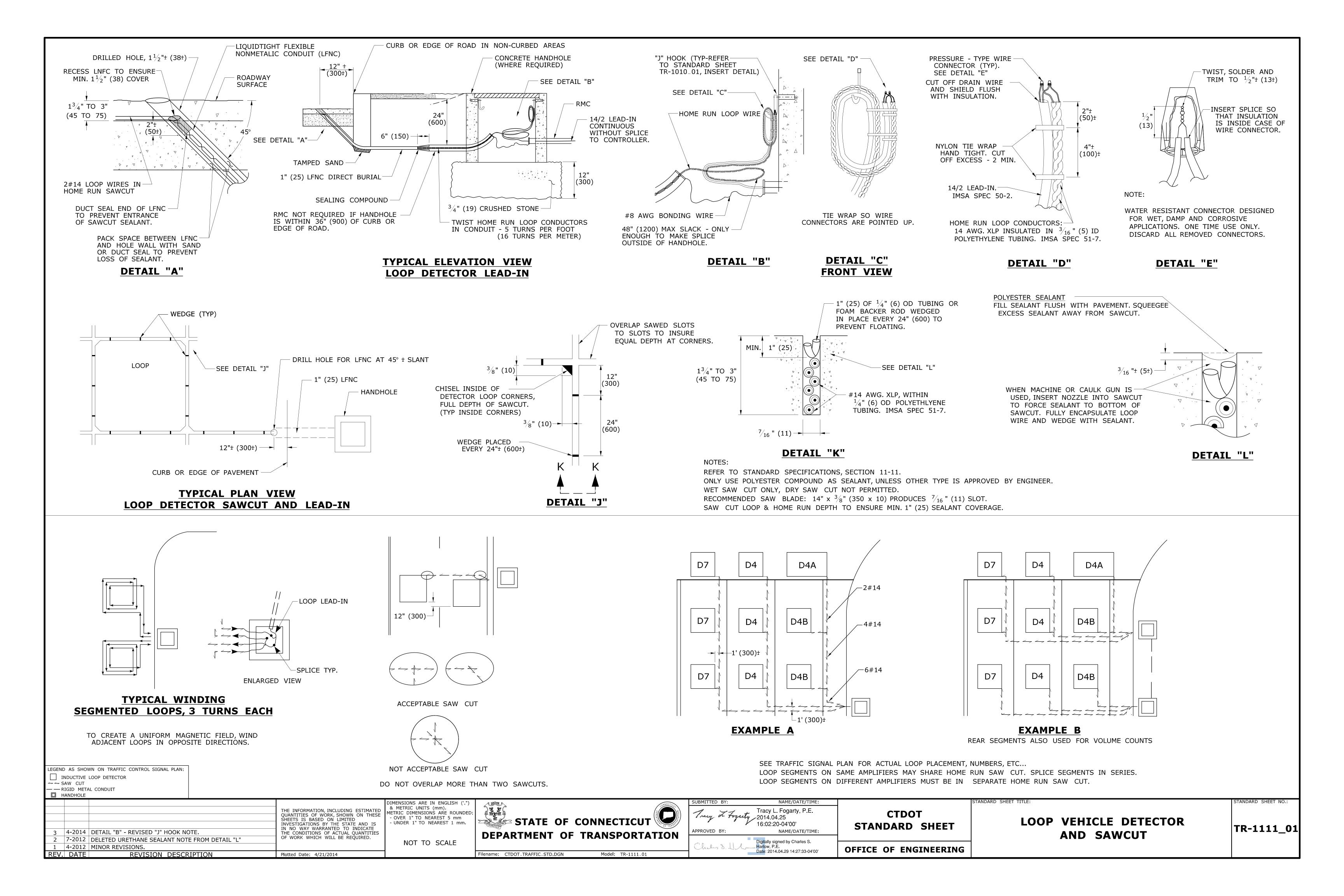
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& METRIC UNITS (mm).
METRIC DIMENSIONS ARE ROUNDED:
- OVER 1" TO NEAREST 5 mm
- UNDER 1" TO NEAREST 1 mm.

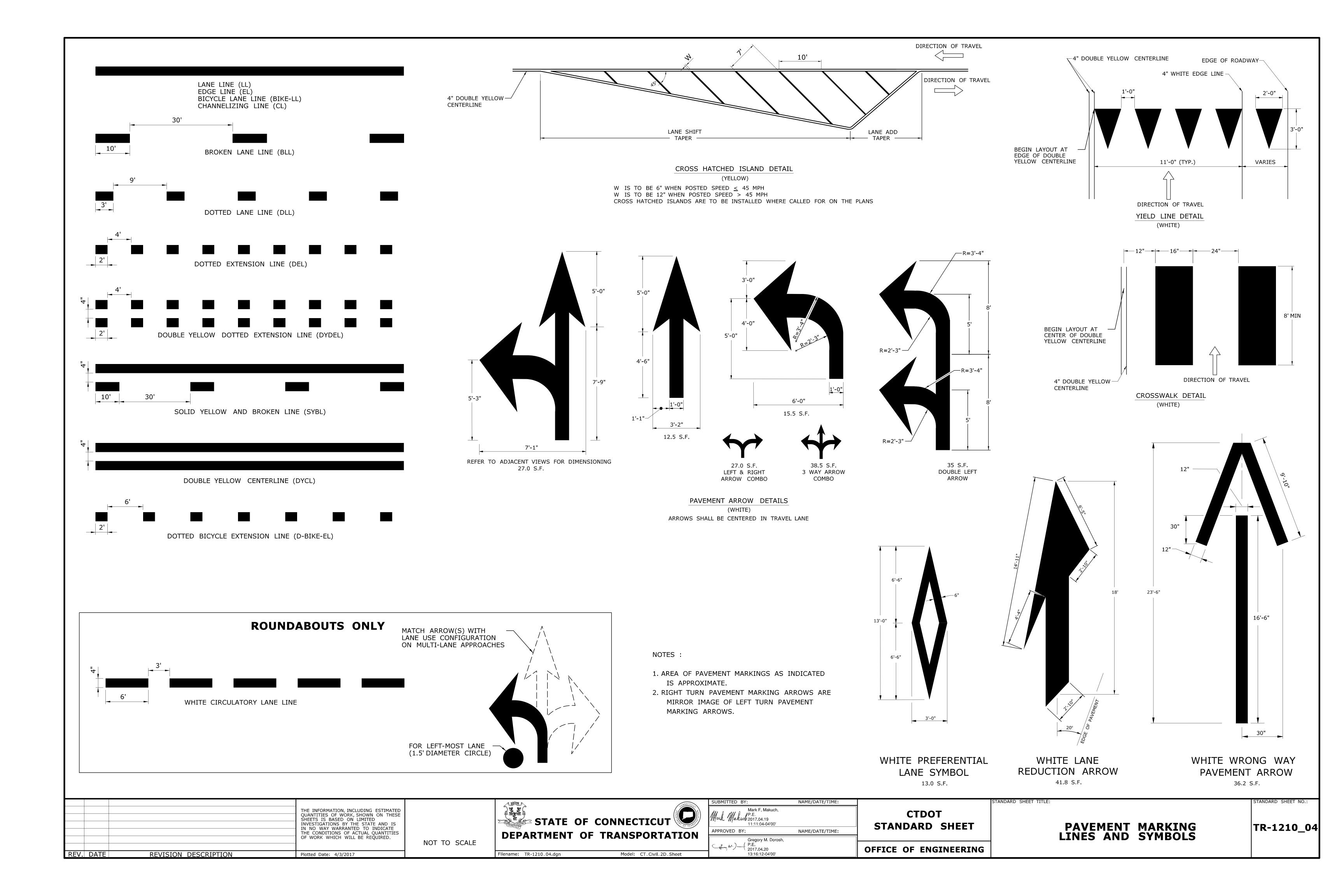
NOT TO SCALE



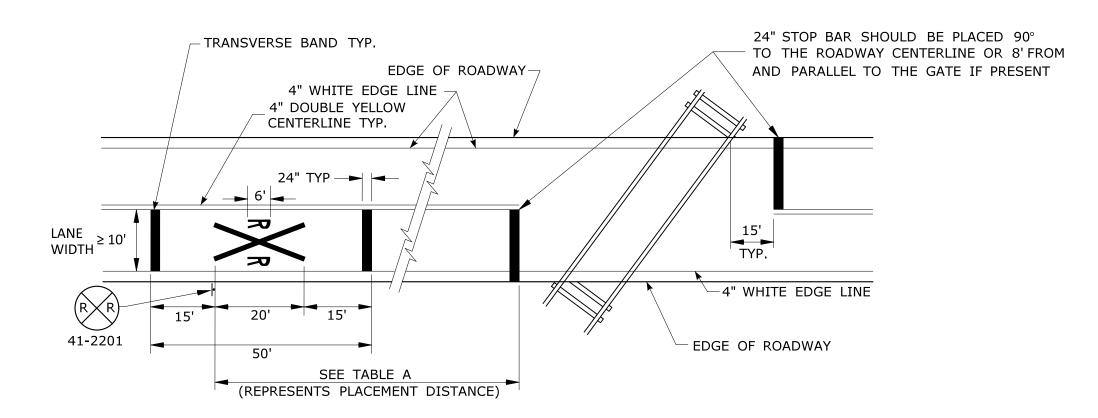
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	Chels S. J	Charles S. Harlow 2013.07.29 14:59:45-04'00'	OFFICE OF ENGINEERING
	Tracy L. Fo	Tracy L. Fogarty 2013.07.29 14:04:24-04'00'	CTDOT STANDARD SHEET
many on Fogury 2013.07.29	SUBMITTED BY:	NAME/DATE/TIME:	

CONTROLLERS	TR-1108_01
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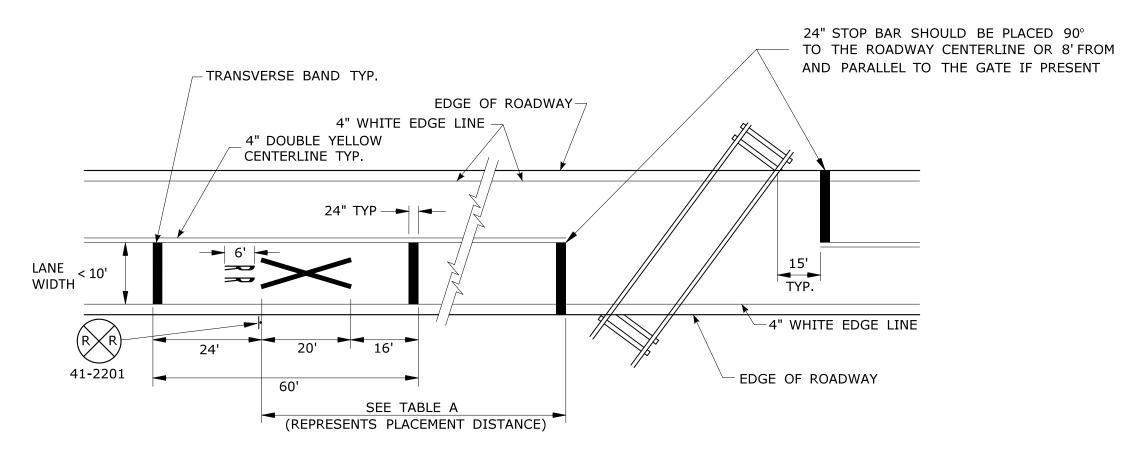




TYPICAL RAILROAD GRADE CROSSING DETAIL (LANE WIDTH ≥ 10')



TYPICAL RAILROAD GRADE CROSSING DETAIL (LANE WIDTH < 10')



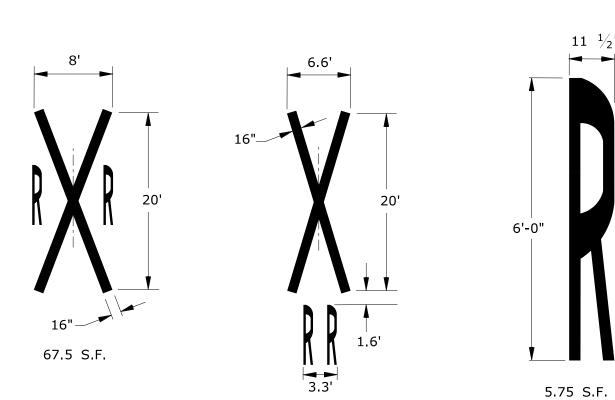


TABLE A	4
POSTED OR 85 PERCENTILE SPEED M.P.H.	MINIMUM DISTANCE FT.
20	100
25	100
30	100
35	100
40	125
45	175
50	250
55	325
60	400
65	475

NOT TO SCALE

NOTES:

GENERAL:

- 1. AREA OF PAVEMENT MARKING SYMBOLS AS INDICATED IS APPROXIMATE.
- 2. REFER TO STANDARD SHEET TR-1210_04 FOR PAVEMENT MARKING LINE DETAILS.

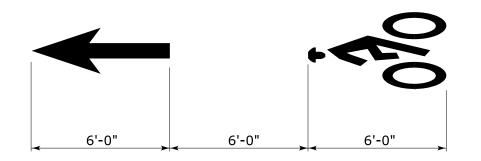
RAILROAD GRADE CROSSINGS:

- 3. RAILROAD MARKINGS SHALL BE WHITE.
- 4. ON MULTI-LANE ROADS THE TRANSVERSE BANDS SHOULD EXTEND ACROSS THE APPROACH LANES AND INDIVIDUAL R X R SYMBOLS SHOULD BE USED IN EACH APPROACH LANE.

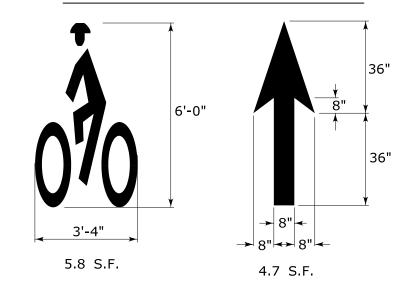
PARKING STALLS:

- 5. AUTOMOBILE ACCESSIBLE PARKING SPACES SHALL BE 15' WIDE INCLUDING 5' OF CROSSHATCH.
- 6) VAN ACCESSIBLE PARKING SPACES SHALL BE 16' WIDE INCLUDING 8' OF CROSSHATCH.
- 7. ACCESS AISLES FOR ANGLED VAN PARKING SPACES SHALL BE LOCATED ON THE PASSENGER SIDE OF THE PARKING SPACE.
- 8. CROSS HATCHED ACCESS AISLES SHALL NOT BE SHARED BETWEEN PARKING SPACES.

TYPICAL LONGITUDINAL SPACING

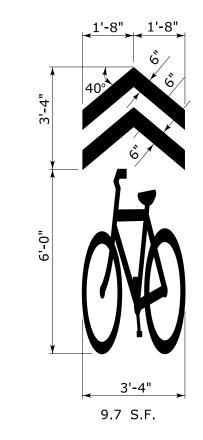


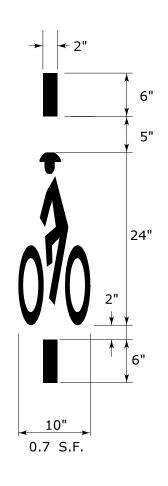
BICYCLE LANE SYMBOL MARKINGS



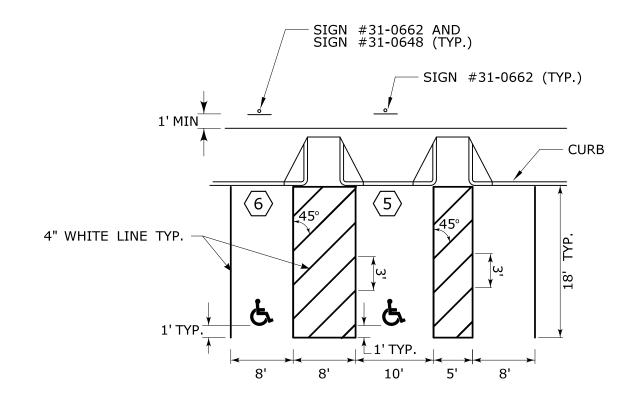
SHARED LANE SYMBOL MARKING

BICYCLE DETECTOR SYMBOL MARKING

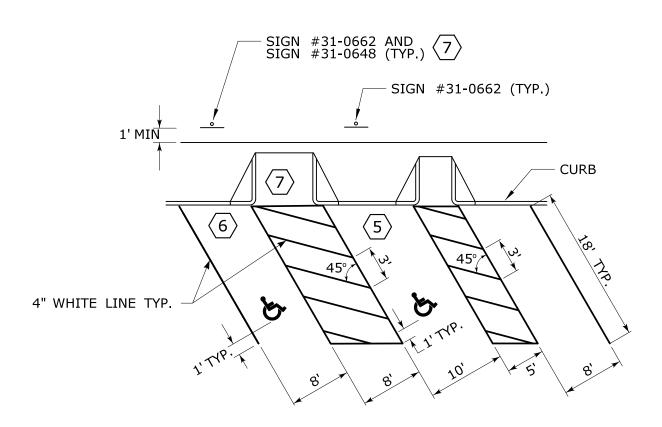




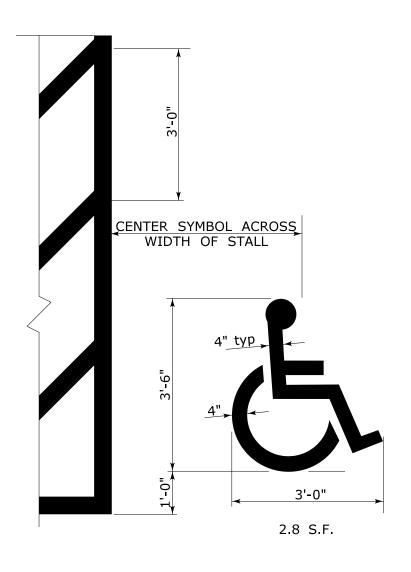
TYPICAL PERPENDICULAR PARKING STALLS DETAIL



TYPICAL ANGLE PARKING STALLS DETAIL



ACCESSIBLE PARKING SPACE SYMBOL



			THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 8/6/2018

67.5 S.F.

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

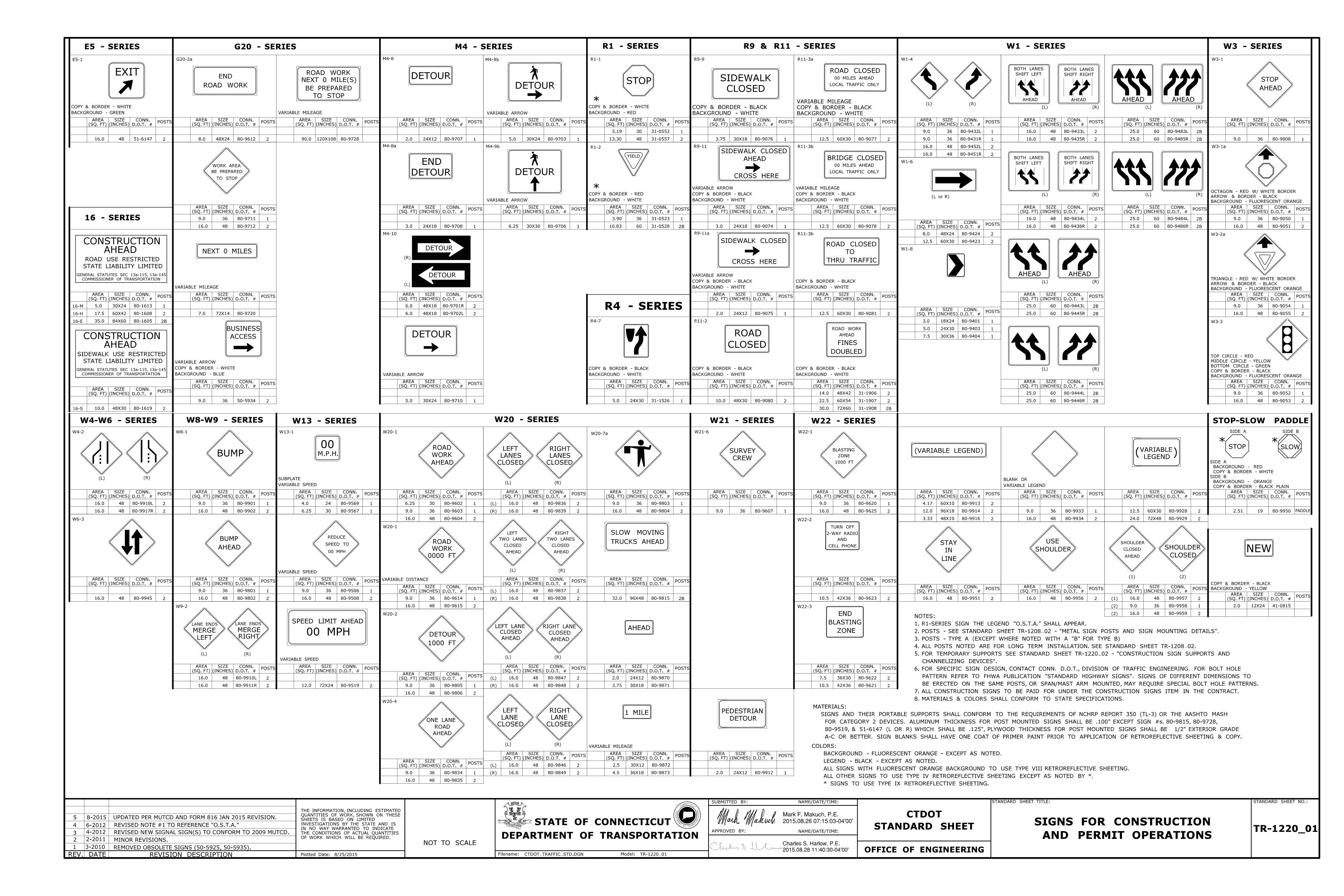
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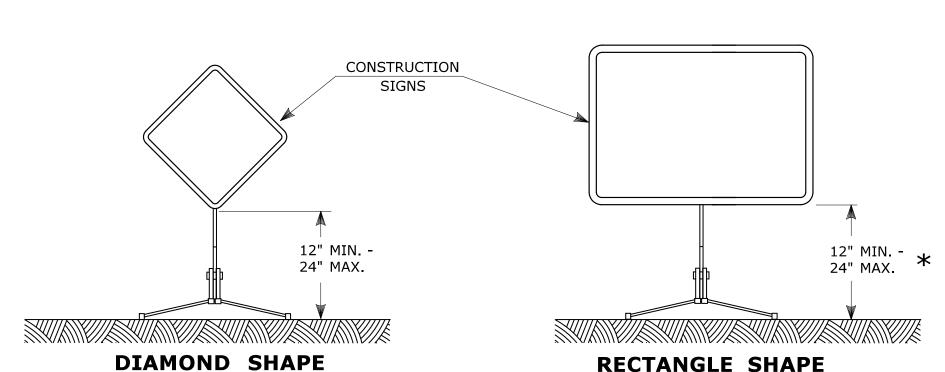
Filename: CTDOT_TRAFFIC_STD_DGN.DGN

		OFFICE OF ENGINEERING
VED BY:	NAME/DATE/TIME:	CTDOT STANDARD SHEET
TTED BY:	NAME/DATE/TIME:	

PAVEMENT MARKINGS FOR BICYCLE LANES, PARKING STALLS, AND RAILROAD GRADE CROSSINGS

TR-1210_09





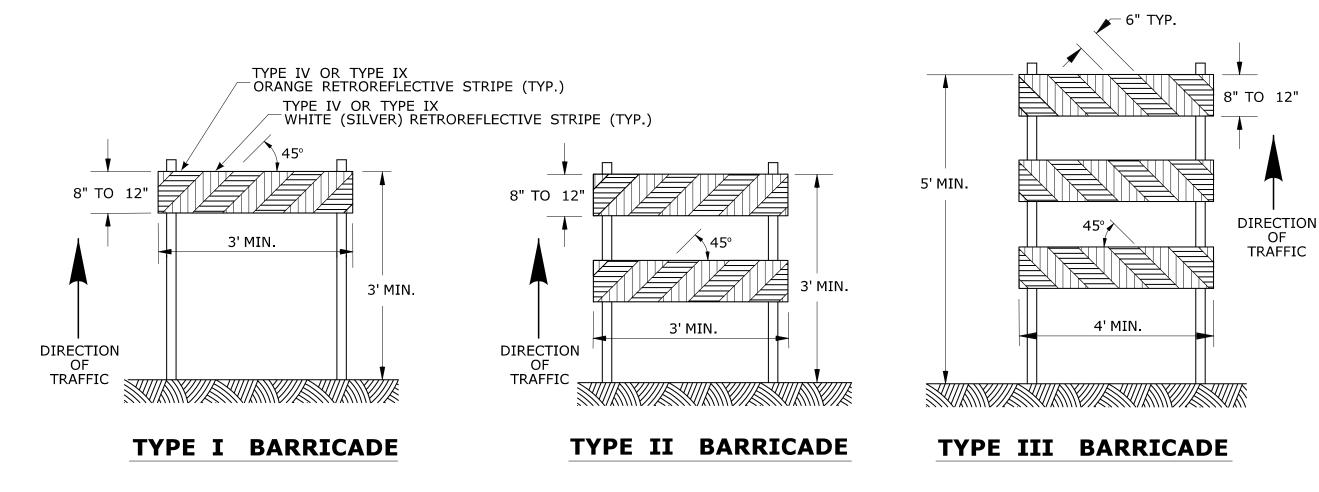
RECTANGLE SHAPE

NOTES FOR PORTABLE SIGN SUPPORTS:

1. SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.

PORTABLE CONSTRUCTION SIGNS

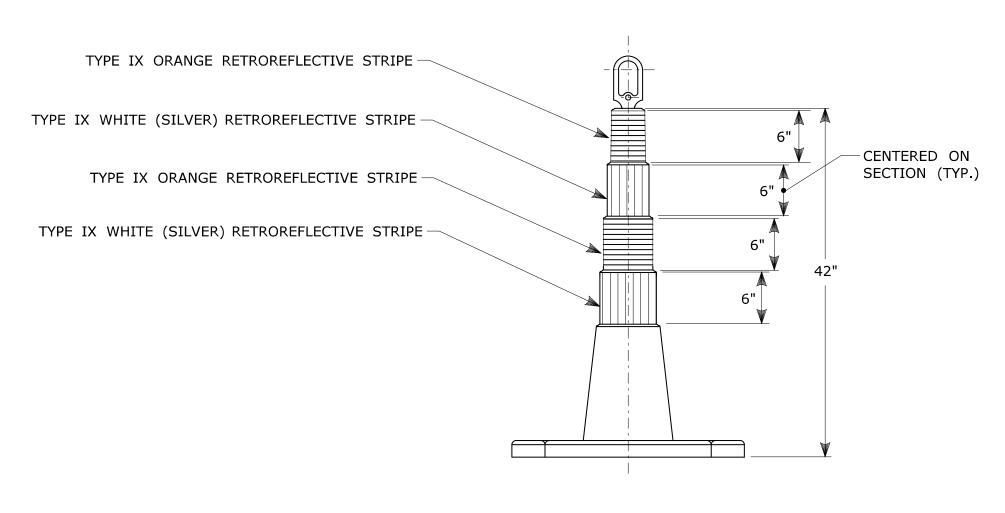
- 2. MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
- 3. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- 4. PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
- 5. PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220_01 "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.
- * FOR E5-1 (EXIT SIGNS) USE MIN 48".



CONSTRUCTION BARRICADES

NOTES:

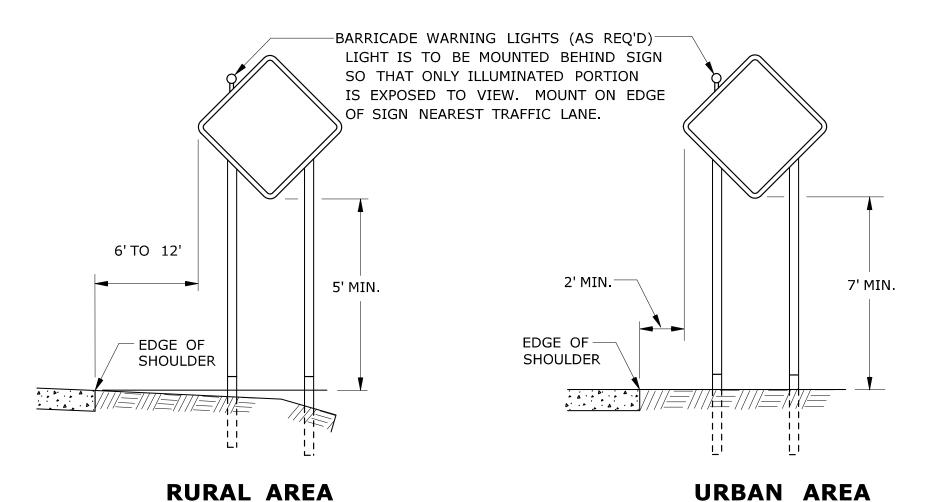
- 1. CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
- 2. MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
- 3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
- 4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- 5. CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- 6. SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



42" TRAFFIC CONE

NOTES:

- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- 2. IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- 3. IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- 4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- 5. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- 6. THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



PLACEMENT OF CONSTRUCTION SIGNS TYPICAL LONG TERM INSTALLATION

NOTES:

SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.

SEE TYPICAL SHEETS:

"TYPICAL SHEETS:

"TYPICAL SIGN SUPPORT AND SIGN PLACEMENT DETAILS-GORE EXIT SIGN"

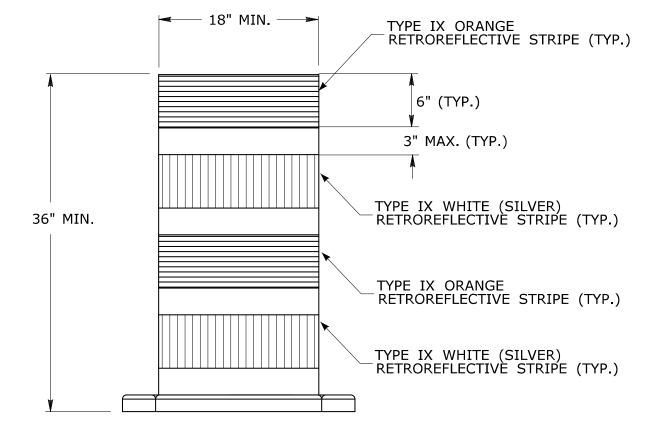
"TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS"

TYPE VI OR TYPE IX WHITE (SILVER) RETROREFLECTIVE STRIPE TYPE VI OR TYPE IX WHITE (SILVER) RETROREFLECTIVE STRIPE 6" 2" 4" 28" MIN

TRAFFIC CONE

NOTES:

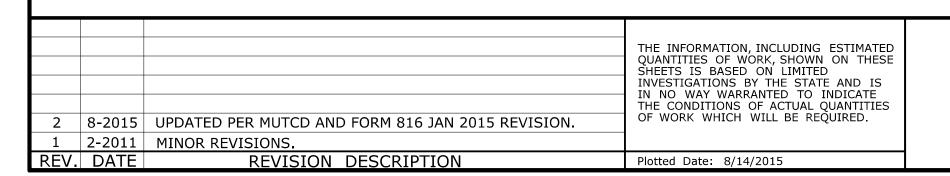
- 1. TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- 2. IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- 3. IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- 4. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- 5. TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
- 6. THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



TRAFFIC DRUM FRONT VIEW

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- 2. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- 3. THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- 4. THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

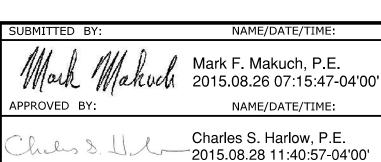


NOT TO SCALE

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Model: TR-1220_02

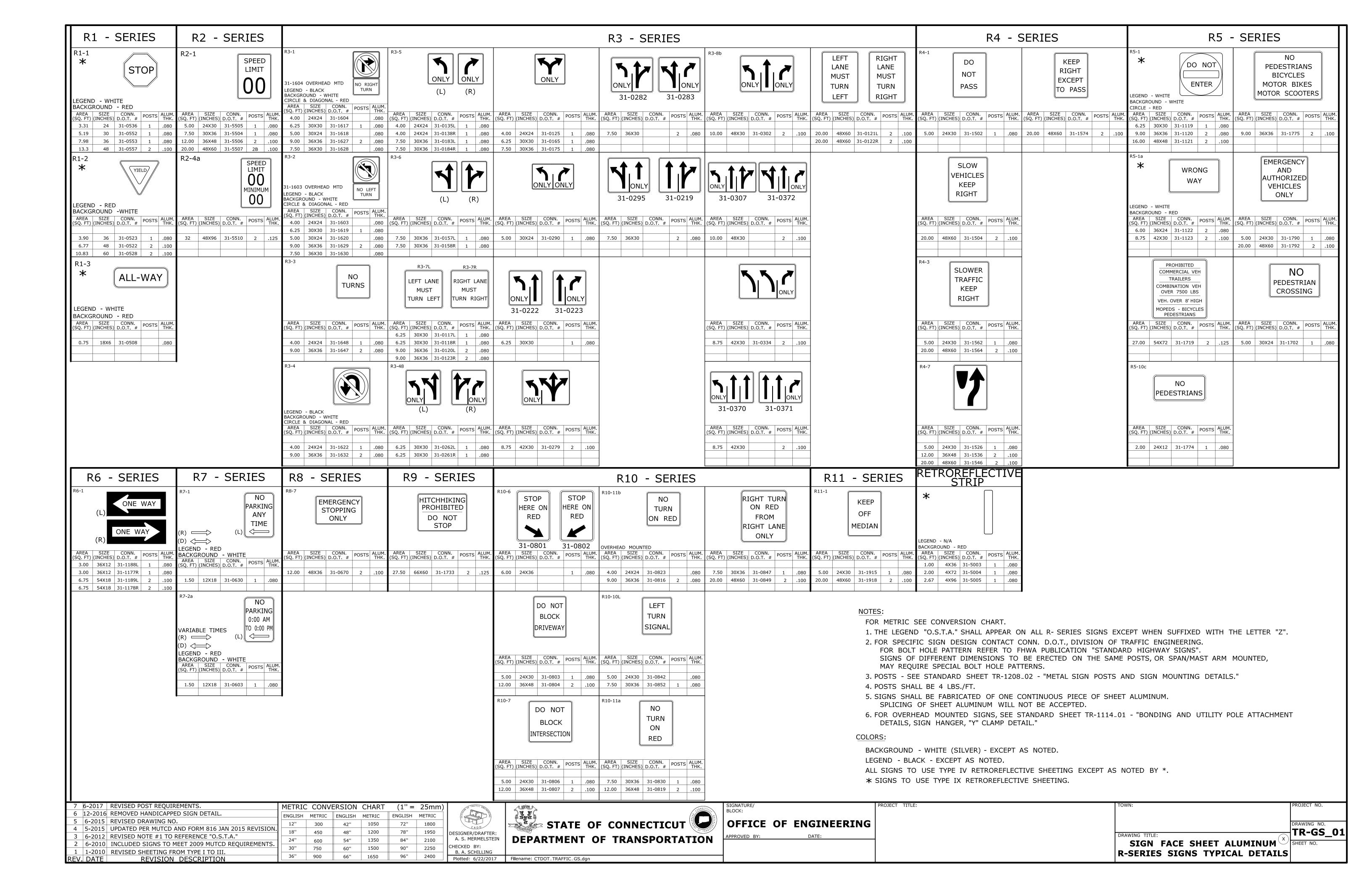


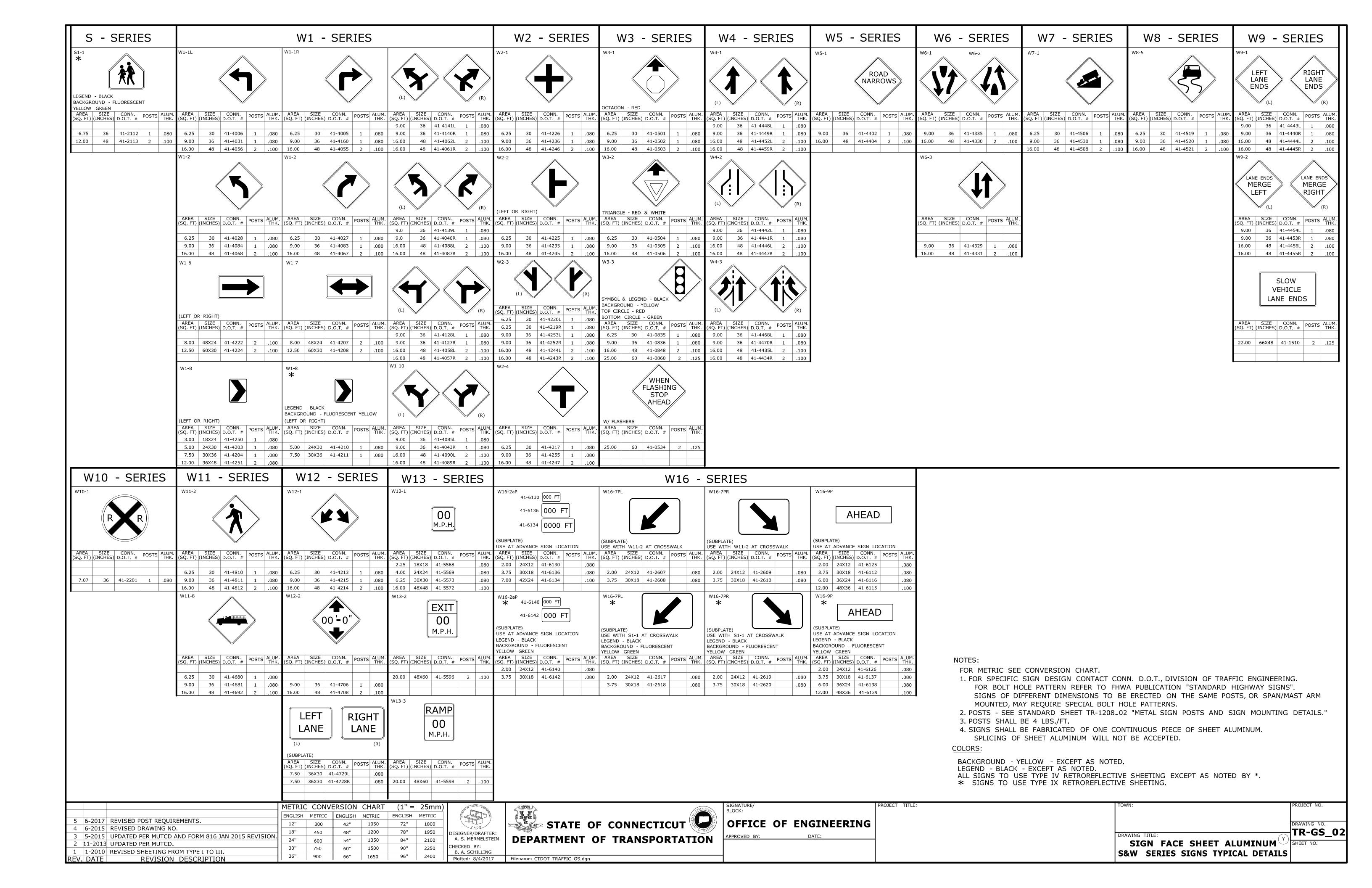


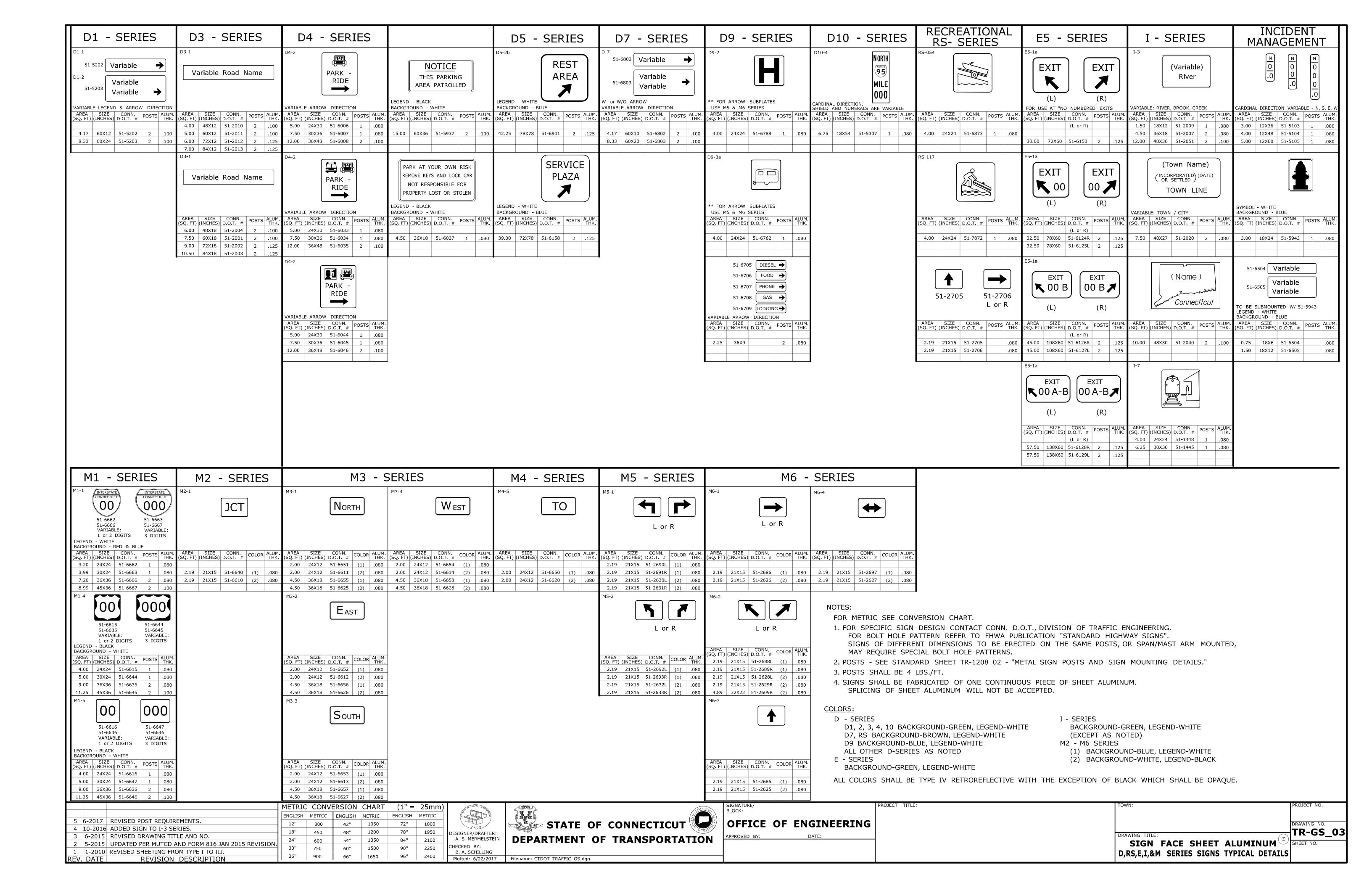
OFFICE OF ENGINEERING

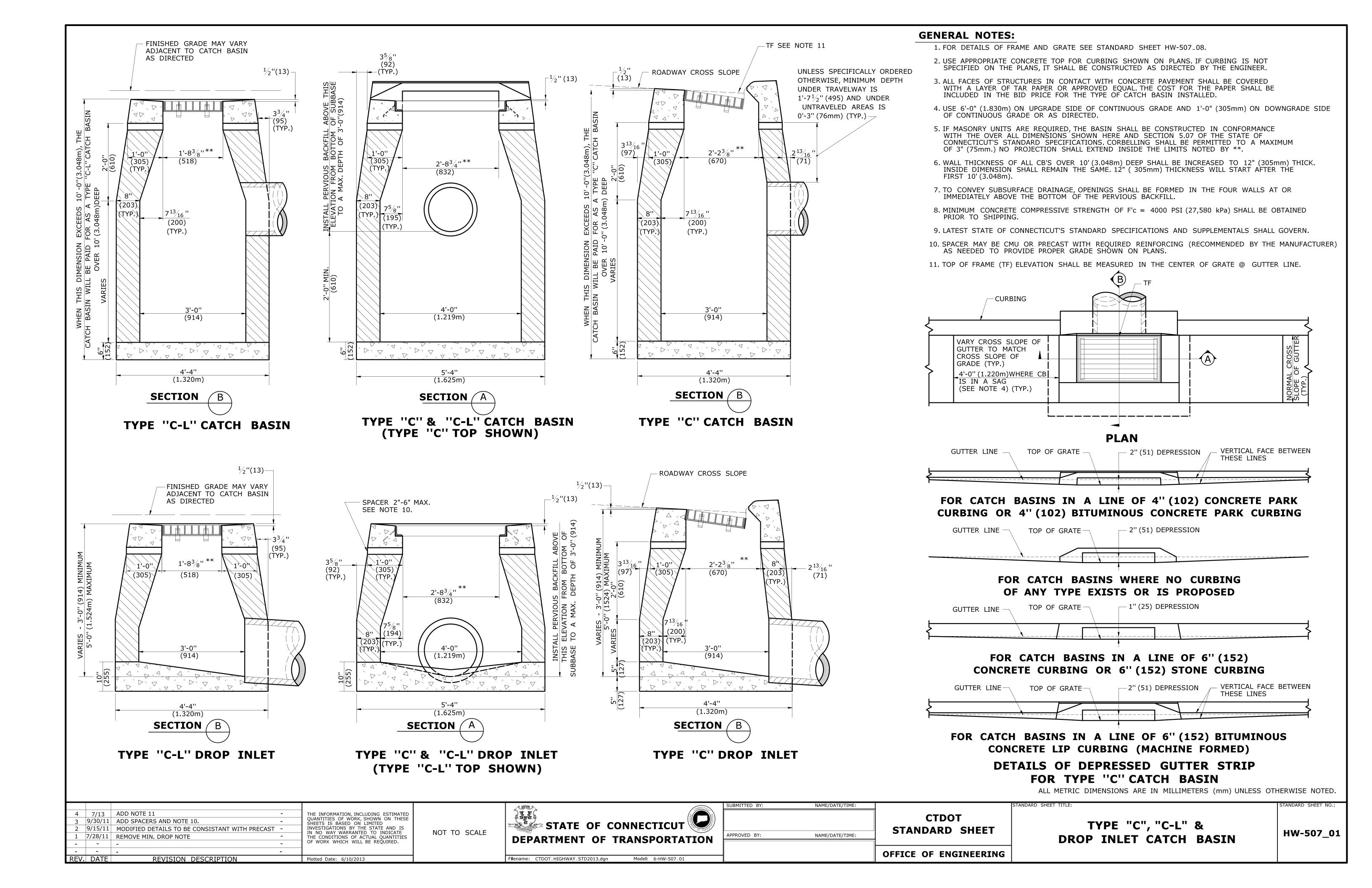
CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES

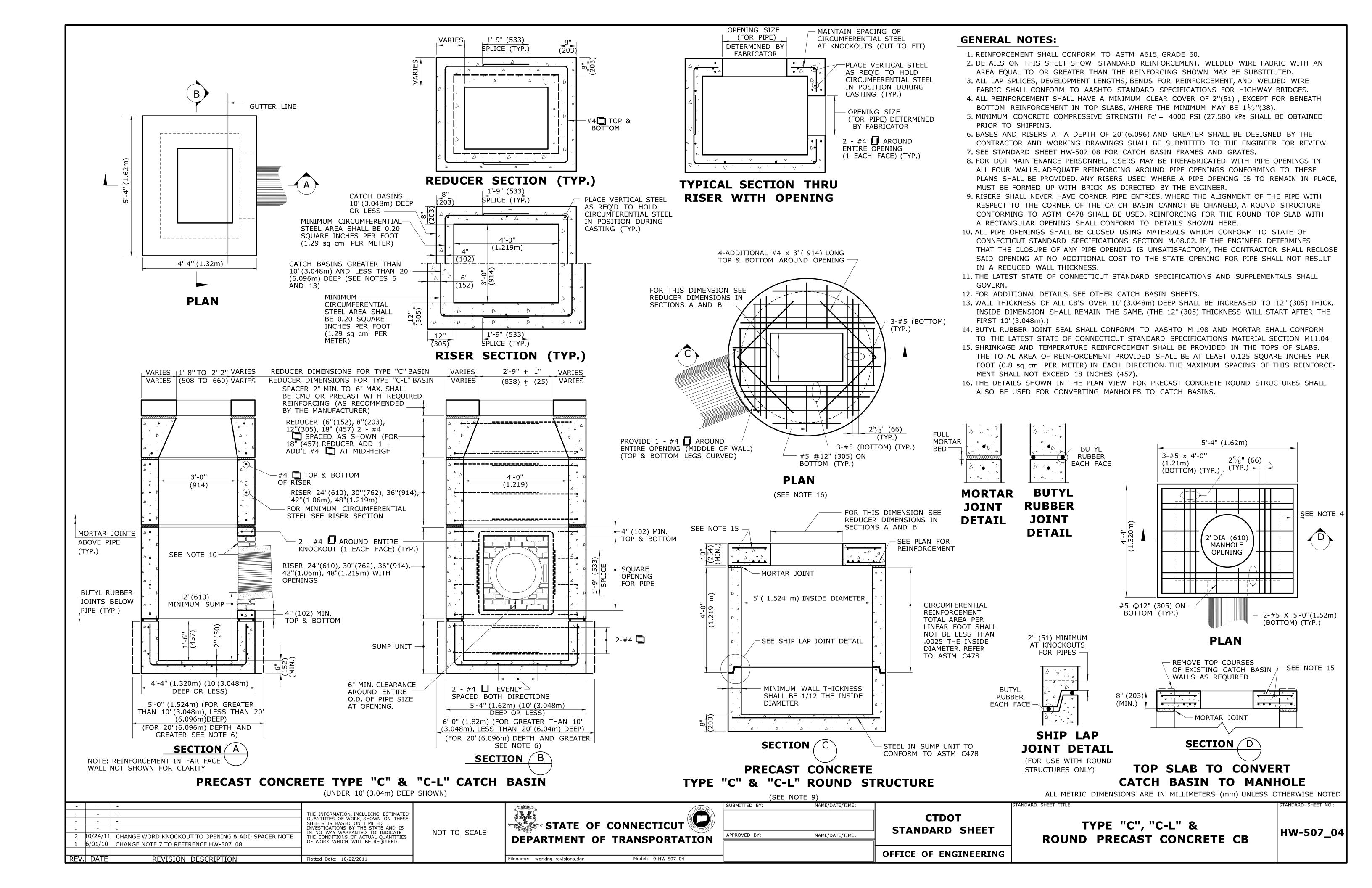
TR-1220_02

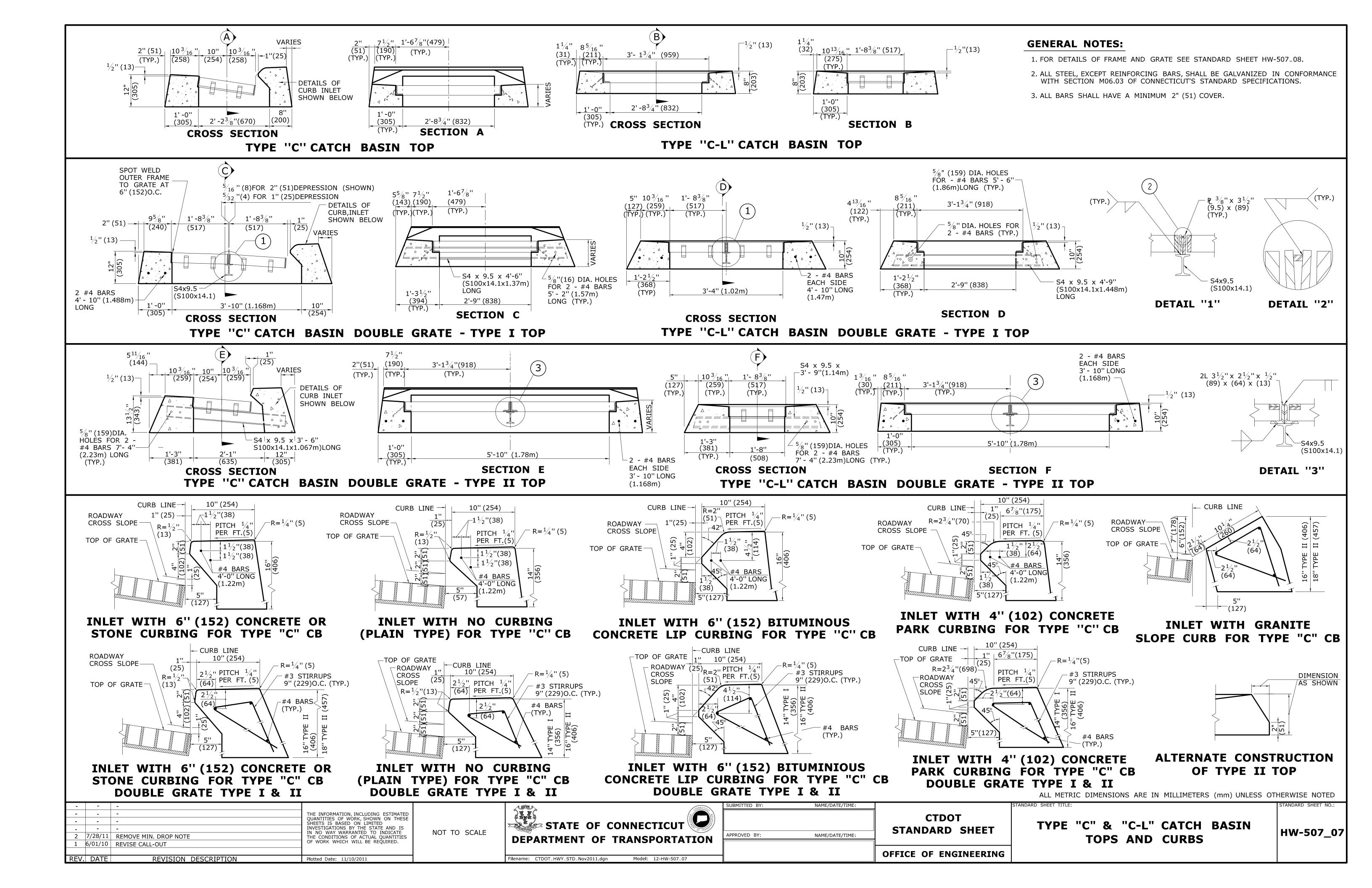


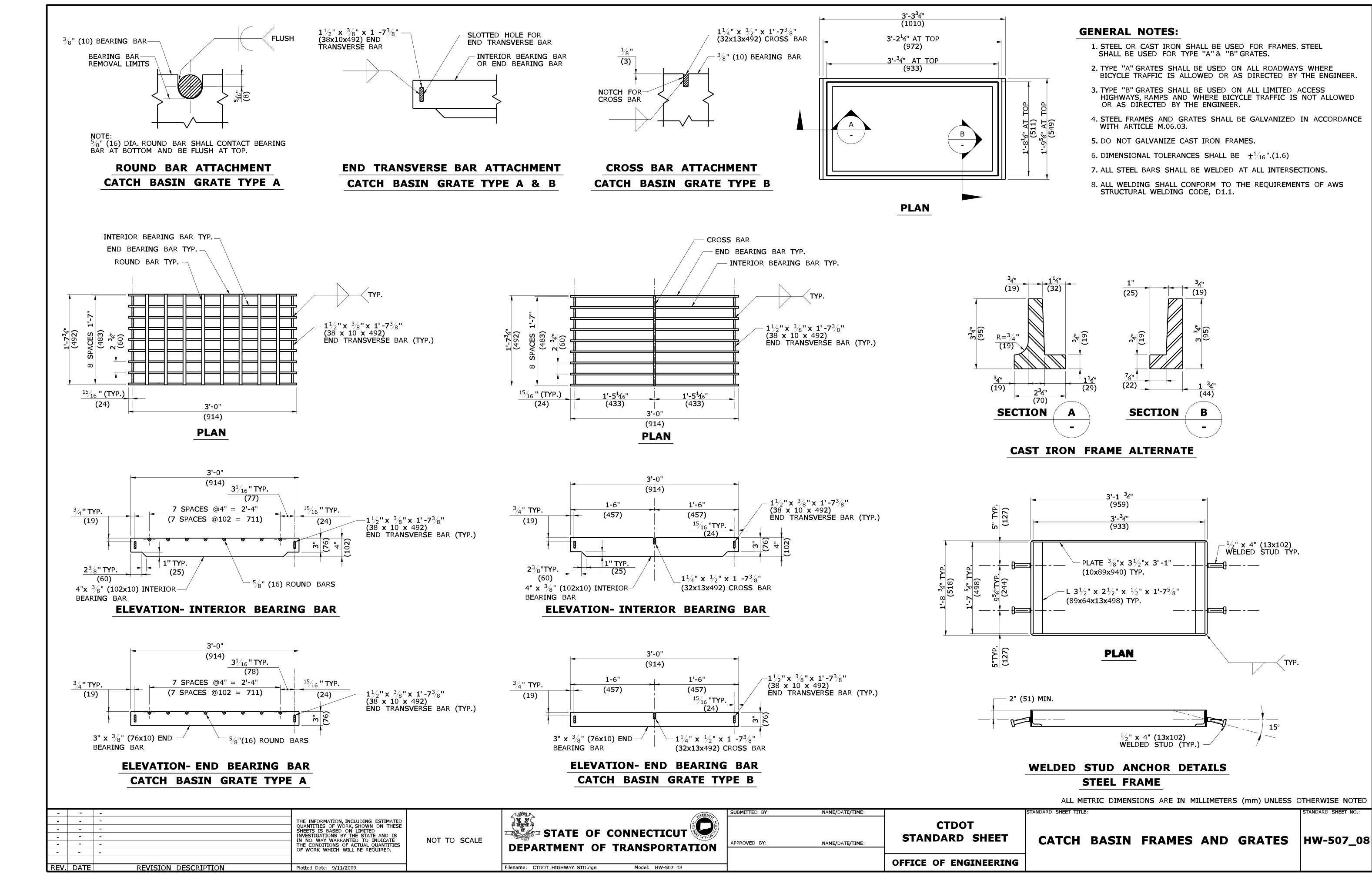


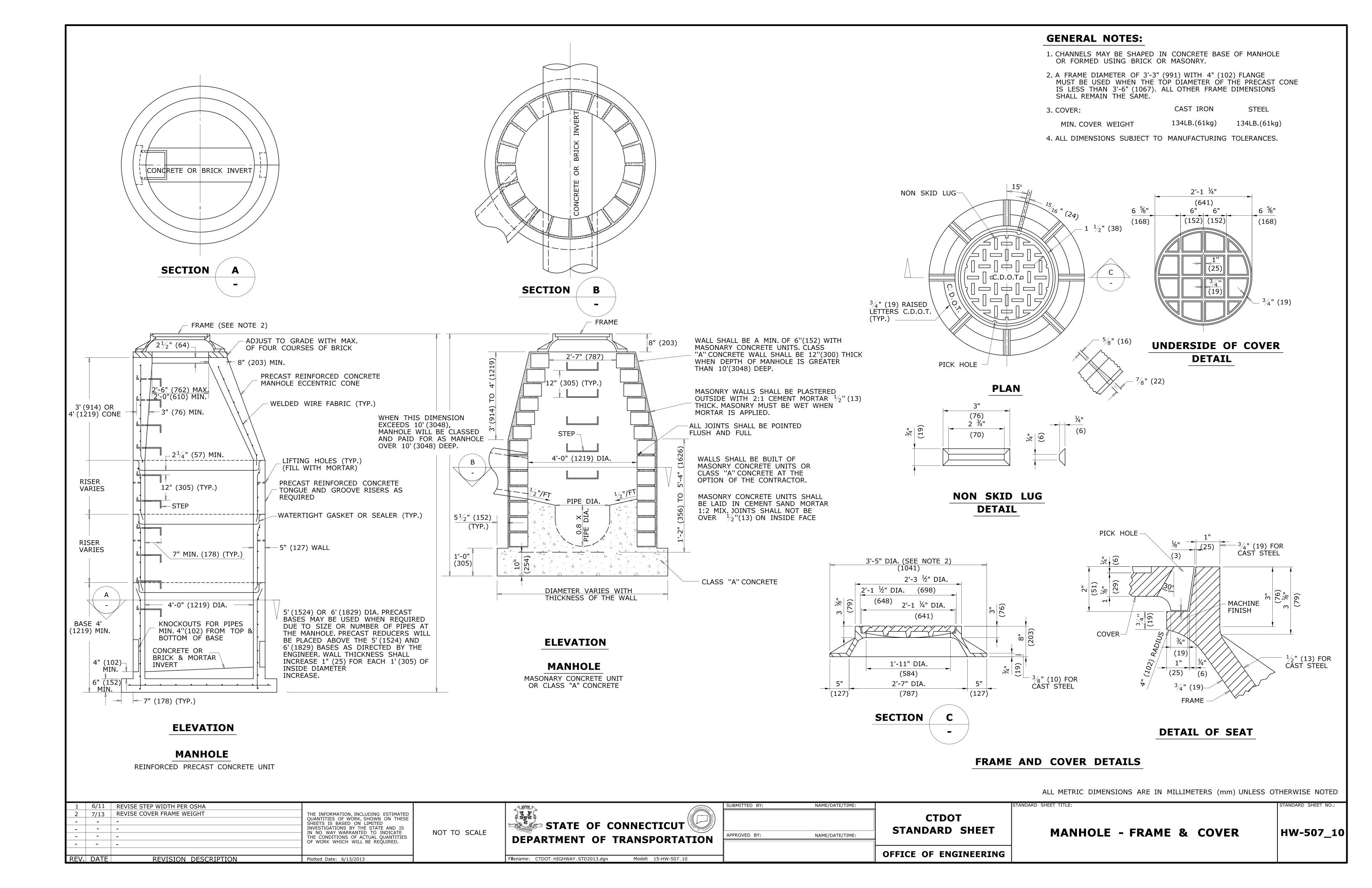


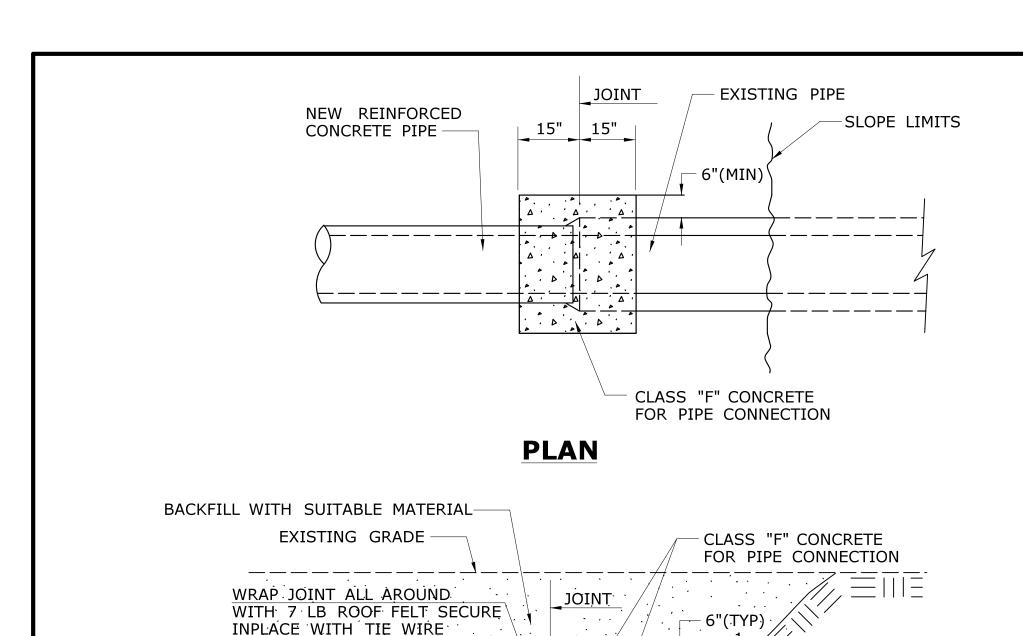












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BEDDING

MATERIAL

PAY LIMIT FOR TRENCH

EXCAVATION AND BEDDING MATERIAL

4 . . 4 . . 4 . . 4 . . 4 . . 4 .

/D= DIA. CIRCULAR PIPE \

EQUIVALENT HORIZONTAL

PIPE TRENCH DETAIL

WHERE GRANULAR FILL IS NOT USED

_& PIPE ARCH OF

\ SPAN

LOWER VERTICAL PAYMENT

AND BEDDING MATERIAL

LIMIT FOR TRENCH EXCAVATION

3/8" MAX. GAP →

SECTION

CONCRETE PIPE CONNECTION

-CUT PIPE WHEN REQUIRED

TYPE II BACKFILL

WITH BEDDING

0.25H

TYPE I BACKFILL WITH BEDDING

PRESHAPE BEDDING

MATERIAL TO 0.10H

PRIOR TO INSTALLING

4"(100) BEDDING/

DEPTH VARIES AS

NOT TO SCALE

DETERMINED BY

THE ENGINEER

MATERIAL-

MATERIAL

MATERIAL

BEDDING MATERIAL - 4"(100)

IN EARTH AND 12"(300)

MINIMUM IN ROCK

NOTES:

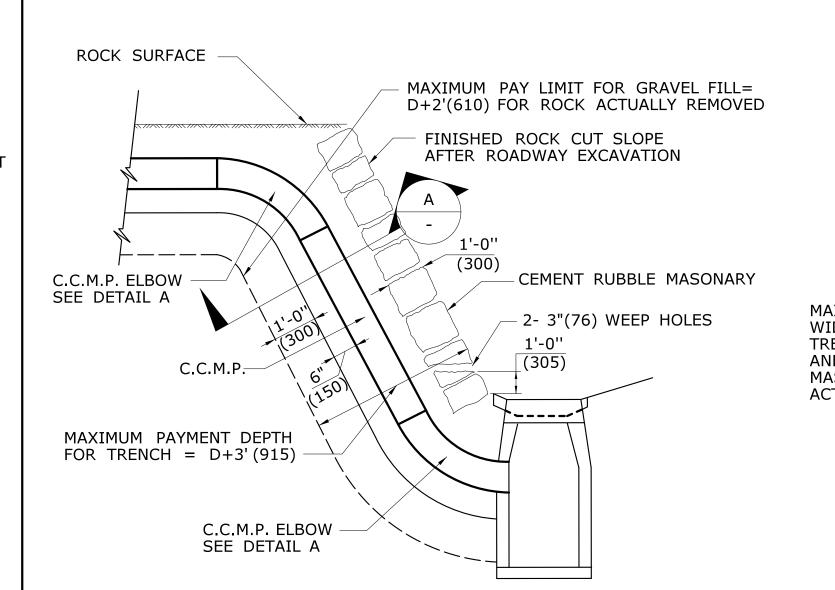
- 1. "CONCRETE PIPE CONNECTION" IS INTENDED FOR USE WHERE A REINFORCED CONCRETE PIPE REPAIR OR MODIFICATION IS NEEDED SOMEWHERE WITHIN A PIPE RUN WHERE A BELL/SPIGOT JOINT CANNOT BE ACHIEVED.
- 2. MAINTAIN INTERIOR ALIGNMENT OF PIPE AT JOINTS UNTIL CONCRETE IS PROPERLY CURED.
- 3. BACKFILL OF PIPE REPAIR WITH SUITABLE MATERIAL MAY NOT TAKE PLACE UNTIL CONCRETE IS PROPERLY CURED.
- 4. CONTRACTOR SHALL MAINTAIN LINE AND GRADE OF PIPE REPAIR OR MODIFICATION BY METHODS APPROVED BY THE ENGINEER.
- 5. HOLES OR GAPS AT JOINT LARGER THAN 3/8" SHALL BE FILLED OR WRAPED TO PREVENT CONCRETE FROM ENTERING PIPE.
- 6. TRENCH EXCAVATION SHALL BE TO THE MAXIMUM EXTENT NEEDED TO PERFORM WORK.

PAY LIMIT FOR TRENCH

EXCAVATION AND BEDDING MATERIAL

/D= DIA. CIRCULAR PIPE

& PIPE ARCH OF EQUIVALENT HORIZONTAL

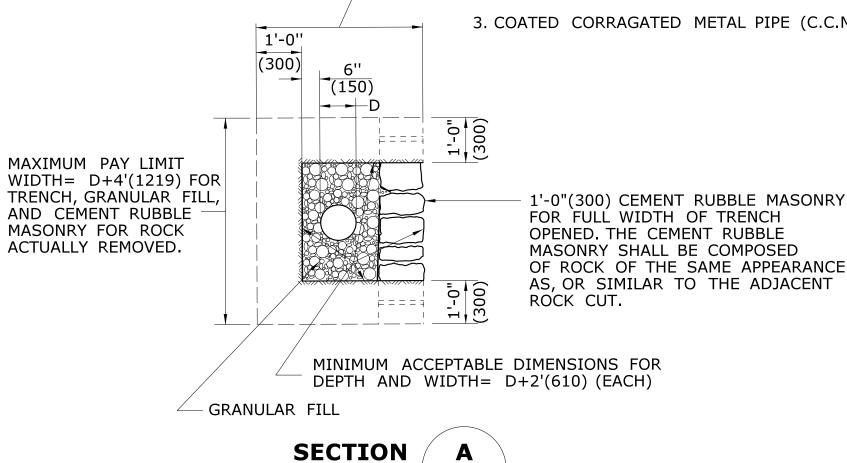


1. ROCK REMOVED BEYOND THE MAXIMUM PAY LIMIT SHOWN SHALL BE REPLACED WITH CEMENT RUBBLE MASONRY AND GRANULAR FILL.

GENERAL NOTES:

2. FILL, AS REQUIRED TO CLOSE THE OPENING AS SHOWN ON THE PLANS, WILL BE AT THE CONTRACTORS EXPENSE. HOWEVER, THE PAY LIMIT LINES MAY BE MODIFIED TO COINCIDE WITH NATURAL FAULTS OR FISSURES OF ROCK AS THE ENGINEER MAY DETERMINE.

3. COATED CORRAGATED METAL PIPE (C.C.M.P.)



MAXIMUM PAY LIMIT DEPTH FOR

FOR ROCK ACTUALLY REMOVED -

TRENCH = D+3'(915). MAXIMUM PAY

LIMIT FOR GRAVEL FILL = D+2'(610)

TYPICAL INSTALLATION OF C.C.M.P. IN ROCK SLOPE

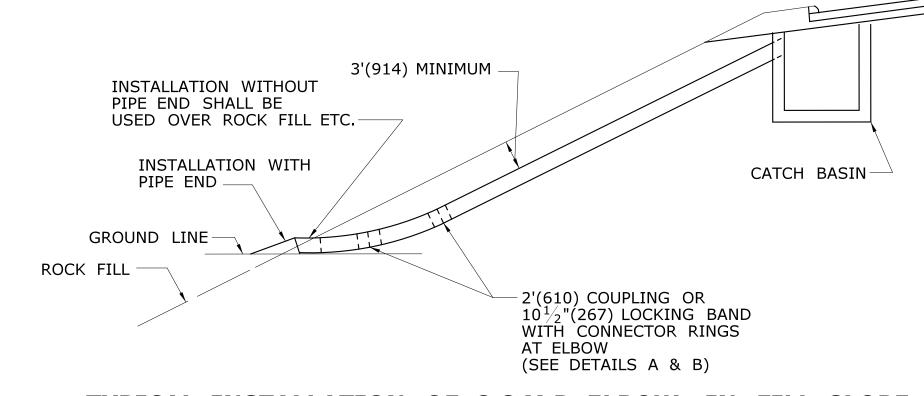


TABLE C CONNECTOD DINCE

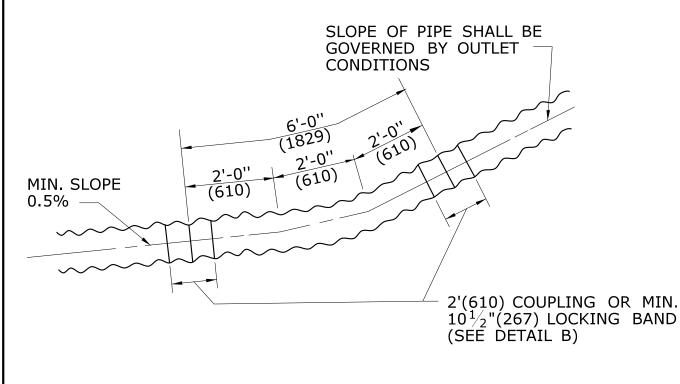
CONNECT	OR RINGS
PIPE DIAMETER 12" (300) 15" (381) 18" (457) 21" (533) 24" (610) 30" (762) 36" (915) 42" (1066) 48" (1219)	LENGTH OF RING 52" (1321) 61" (1549) 71" (1803) 80" (2032) 90" (2286) 108" (2743) 128" (3251) 147" (3734) 166" (4216)

 $10\frac{1}{2}$ "

(267)

1/// 1111/14 | 111/14/1/

TYPICAL INSTALLATION OF C.C.M.P ELBOW IN FILL SLOPE



CIRCUMFERENTIALLY ALTERNATE CONNECTOR CORRUGATED ENDS RING RODS: $\frac{7}{16}$ "(11) DIAMETER ELECTRO-GALVANIZED WITH 6"(152) LENGTH OF ½"(13) ROLLED THREADS EACH END, FURNISHED CURVED, TO FIT PIPE. SEE TABLE C

CONNECTOR RINGS

LUGS: DOUBLE TAKE UP, CAST IRON, ELECTRO-GALVINIZED.

(267)

NOTE: THE COUPLER FASTENING DEVICE SHALL NOT INTERFERE WITH INSTALLATION OF CONNECTOR RINGS.

> **DETAIL B ELBOW DIMENSIONS**

PIPE TRENCH DETAIL WHERE GRANULAR FILL IS USED AS BEDDING

Filename: CTDOT_HIGHWAY_STD2013.dgn

LOWER VERTICAL

PAYMENT LIMIT

FOR TRENCH

EXCAVATION

DETAIL A C.C.M.P. ELBOW AND COUPLING

ALL METRIC DIMENSIONS ARE IN MILLIMETERS (mm) UNLESS OTHERWISE NOTED

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED - | - | -INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE 7/13 ADD CONCRETE PIPE CONNECTION DETAIL 2 | 6/01/10 | REVISE TITLE TO SAY TRENCH DETAIL THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED. 6/01/10 | REMOVE GRAVEL, REPLACE W/ GRANULAR REV. DATE REVISION DESCRIPTION Plotted Date: 6/13/2013

4 . 4 . 4 . 4

STATE OF CONNECTICUT **DEPARTMENT OF TRANSPORTATION**

-TYPE II BACKFILL

TYPE I BACKFILL

WITH BEDDING

MATERIAL

PRESHAPE BEDDING

MATERIAL TO 0.10H

PRIOR TO INSTALLING

WITH BEDDING

0.25H

LOWER VERTICAL

PAYMENT LIMIT

FOR BEDDING

MATERIAL

Model: 17-HW-651_01

GRANULAR FILL

MATERIAL

JBMITTED BY: NAME/DATE/TIME: **CTDOT** STANDARD SHEET APPROVED BY: NAME/DATE/TIME: OFFICE OF ENGINEERING

C.C.M.PIPE INSTALLATIONS IN FILL & **ROCK SLOPES & PIPE TRENCH DETAIL**

HW-651_01

