

# The Hollow NRZ Plan

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Bridgeport, CT



*The  
Hollow*



**FINAL DRAFT - March 20, 2017**

Amended April 4, 2022

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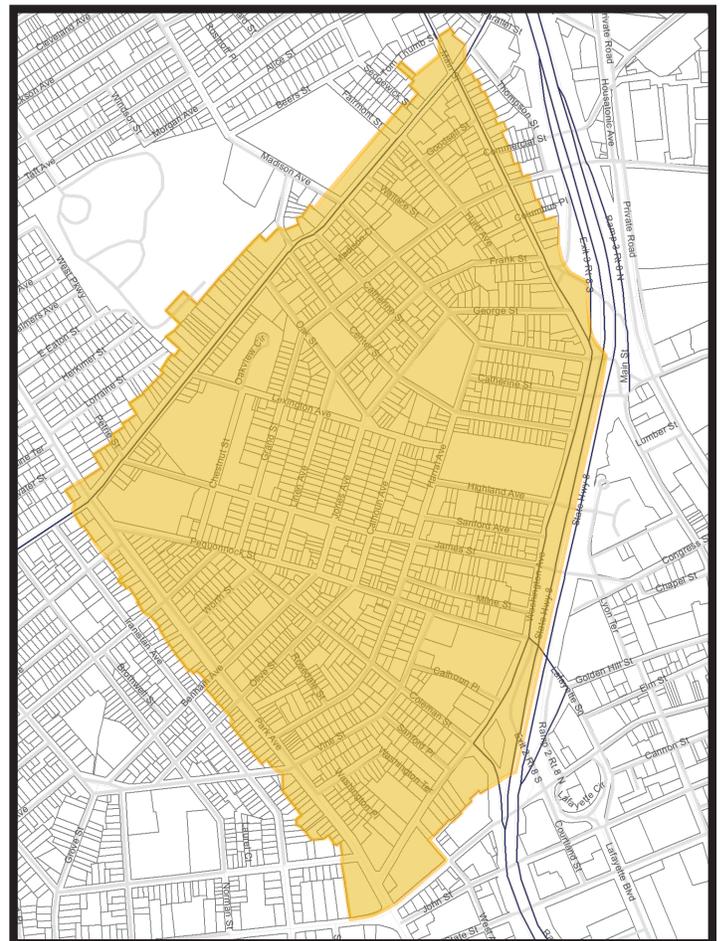
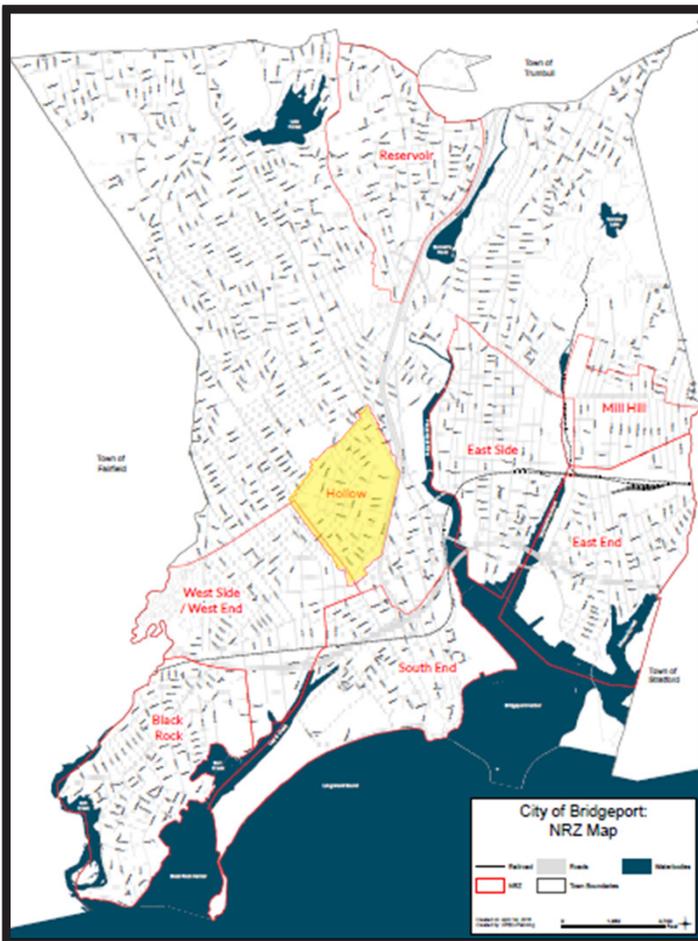
# Hollow NRZ Plan

## 1. Purpose and background of the Hollow NRZ Strategic Plan

In partnership with the Hollow Neighborhood Revitalization Zone (NRZ), Vita Nuova, LLC began work in September of 2016 to create a new comprehensive redevelopment plan for the Hollow neighborhood in Bridgeport, Connecticut. The Hollow NRZ was designated in 2004, and the first comprehensive plan for the Hollow was adopted in 2005. The goal of this plan is to provide the NRZ with an updated action plan for neighborhood improvements based on current conditions, community input, and recommendations from planning professionals.

### A. Neighborhood Location and Boundaries:

The Hollow is located in the center of Bridgeport, and is considered the smallest neighborhood with a total area of .42 square miles.



The boundary of the Hollow has followed more of a roadway definition that is reflected in both topography and old railroads or trolley lines that are now gone. The streets that currently make up the boundary of the Hollow are Park Avenue (on the west), North Avenue (on the north), Main Street (on a portion of the east) and Washington Avenue (on the east and south).

## **B. The Hollow Strategic Planning Process**

### **1. Community Meeting**

On September 26th, 2016 members of the Hollow NRZ and Vita Nuova, LLC and PRE/view Landscape Architects met at the City Hall in Bridgeport to conduct an initial assessment of the Hollow and the topic areas the NRZ members hoped to see addressed in the plan. The workshop began by reviewing and confirming the boundaries of the Hollow, followed by a discussion about the existing neighborhood conditions. Members of the NRZ outlined the primary challenges in the Hollow, and which problems they wanted the plan to address. A process for conducting community interviews was agreed upon, and a schedule was set for the remainder of the planning process.

### **2. Community Interviews**

In order to gain more community input, members of the Hollow community administered a survey by phone or in person. Surveys were conducted from October 1st through November 28th, 2016. An initial list of 39 community members were sought out for participation in the survey. After attempts to communicate with all potential participants, about 25% of the community members completed the full survey.

#### **Survey Content:**

The survey included initial questions about the participant's demographic information, time spent living or working in the Hollow, and occupation.

Participants then ranked their concern regarding a variety of topics: public safety, walkability/sidewalks, traffic congestion, vacant properties, blight, parking, litter and sanitation, community identity, community centers, and outdoor space.

Participants were then asked to list their top 3 concerns for the Hollow and their top three needs for their business, organization, or family. Finally, the survey included information on how the participant could become more involved in the process of creating the new NRZ plan.

## **Survey Results**

### **Demographic Information**

5 people ages 28 – 50

3 people ages 51 – 65

1 person ages 66 – older

5 Male

4 Female

4 Black (African American)

3 Hispanic, Latino, Caribbean

3 White (Caucasian)

### **Years Lived or Worked in the Hollow**

2 people for 4 - 7 years

2 people for 8 - 11 years

1 person for 12 – 20 years

4 people for 21+ years

## Survey Results (continued)

### Average Ranking of Neighborhood Concerns

(1 = Excellent and 10 = Very Bad)

1. Availability of Trash Cans (8.3)
2. Availability of Parking (7.8)
3. Litter/Sanitation/Trash Collection (7.8)
4. Community or Neighborhood Identity (6.8)
5. Traffic/Congestion (6.4)
6. Public Safety (6.2)
7. Outdoor/Recreation Space (6.2)
8. Walkability/Sidewalks (6)
9. Blight/Graffiti (6)
10. Vacant Property and Buildings (5.1)

### Top Three Issues for Survey Participants:

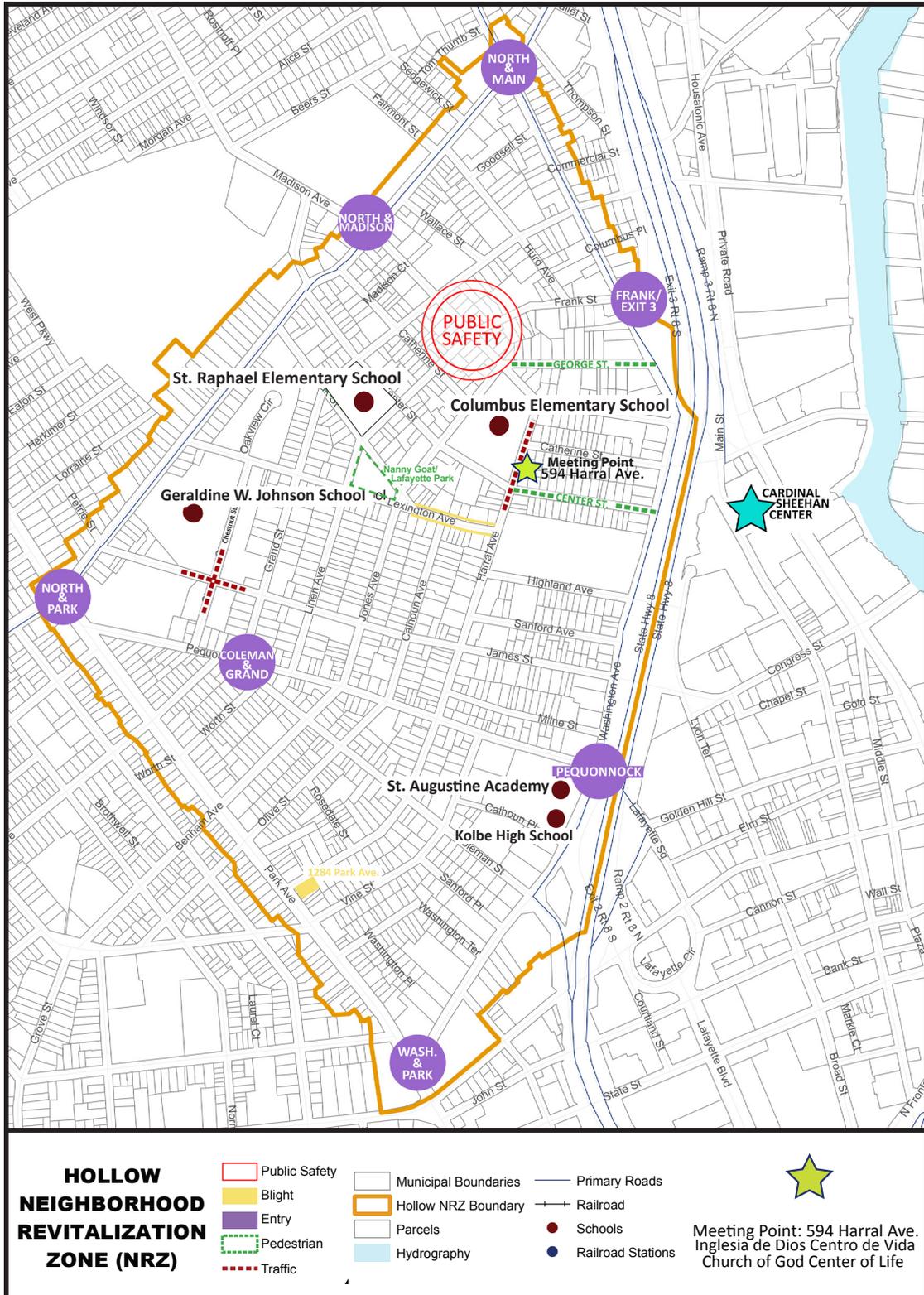
1. Safety (mentioned 8 times)
2. Trash/Cleanliness (mentioned 3 times)
3. Blight/Vacancies (mentioned 3 times)
4. Landlord/Renter Issues (mentioned 3 times)
5. Parking (mentioned 2 times)
6. Community Image/Cohesiveness (mentioned 2 times)
7. Beautification (mentioned 1 time)
8. Congestion (mentioned 1 time)
9. High Taxes (mentioned 1 time)
10. Relationship with the City of Bridgeport (mentioned 1 time)
11. Sanctuary City Status (mentioned 1 time)
12. Poverty (mentioned 1 time)

### Top Three Needs of Survey Participants

1. Safety (mentioned 3 times)
2. Parking (mentioned 2 times)
3. Less Density (mentioned 2 times)
4. Trash/Cleanliness (mentioned 1 time)
5. Additional Park Space (mentioned 1 time)
6. More Plowing in Winter (mentioned 1 time)
7. Fewer Convenience Stores and More Major Retailers (mentioned 1 time)
8. Community Cohesion (mentioned 1 time)
9. Resource Center for Children (mentioned 1 time)
10. Increased Landlord Responsibility (mentioned 1 time)
11. Plantings/Gardens (mentioned 1 time)

### 3. Walking Tour

On October 15th, 2016, the Hollow NRZ members of Vita Nuova, LLC, PRE/view Landscape Architects, and other community members met at the Church of God & Light on Harral Ave in Bridgeport to take a walking tour of the Hollow and further discuss topics to be addressed by the plan. A flier was designed to advertise the event, and was distributed throughout the Hollow by members of the NRZ prior to the meeting. During the tour both broader issues and opportunities were discussed and specific locations were noted.





#### 4. **Framework Meeting**

On November 28th, 2016 the Hollow NRZ team and community members met at City Hall in Bridgeport for a presentation on the framework of the plan and background information on the Hollow. Members of Vita Nuova team presented the outline of the plan, demographic data for the Hollow, and the results from community surveys. The next steps for the completion of the plan were confirmed.

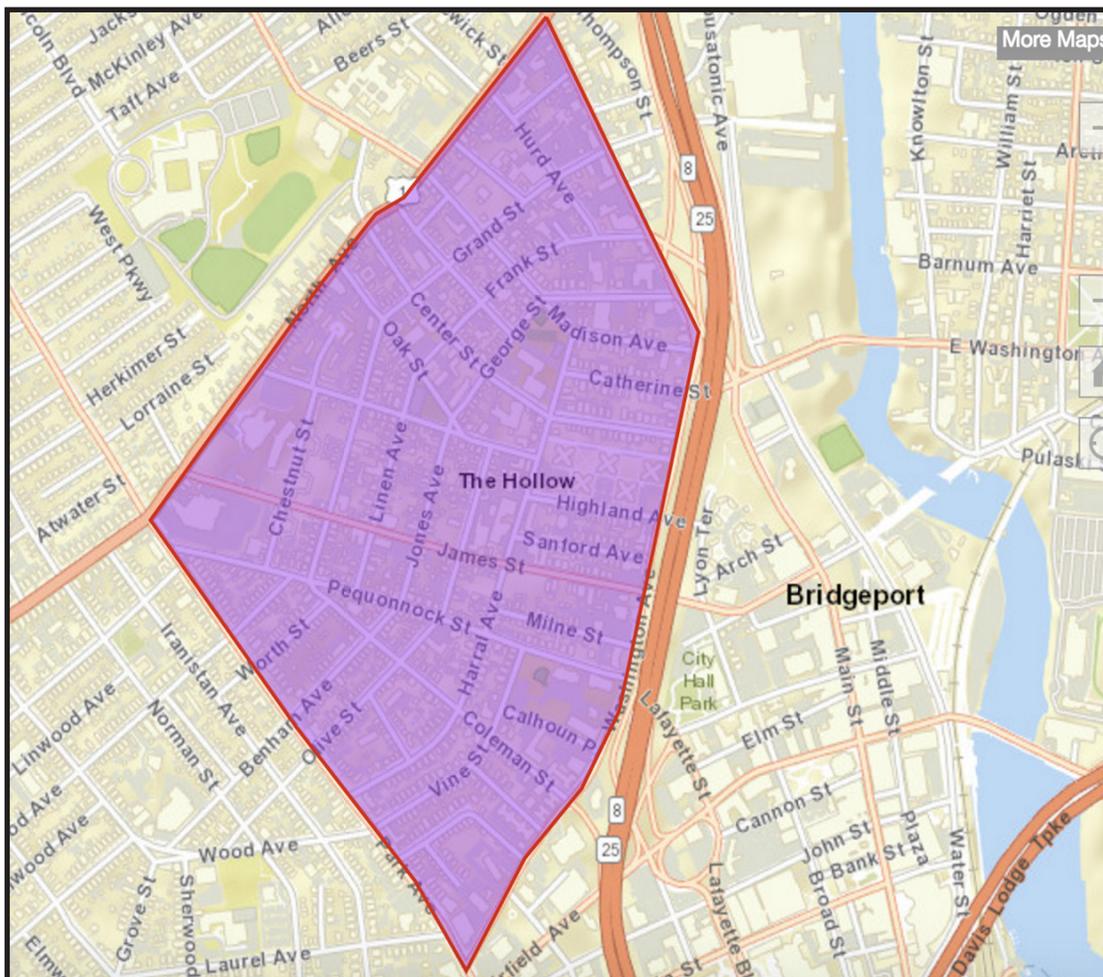
#### 5. **Solutions Scoping Meeting**

On January 30th, 2016 members of Vita Nuova, LLC, PRE/view Landscape Architects, and the Hollow NRZ met at the City Hall in Bridgeport to review the categories of problems to be addressed, and to introduce possible solutions. Feedback was given by community members, and a report on street parking formations was presented.

## 2. **Community Profile**

### A. **Demographic and Economic Trends**

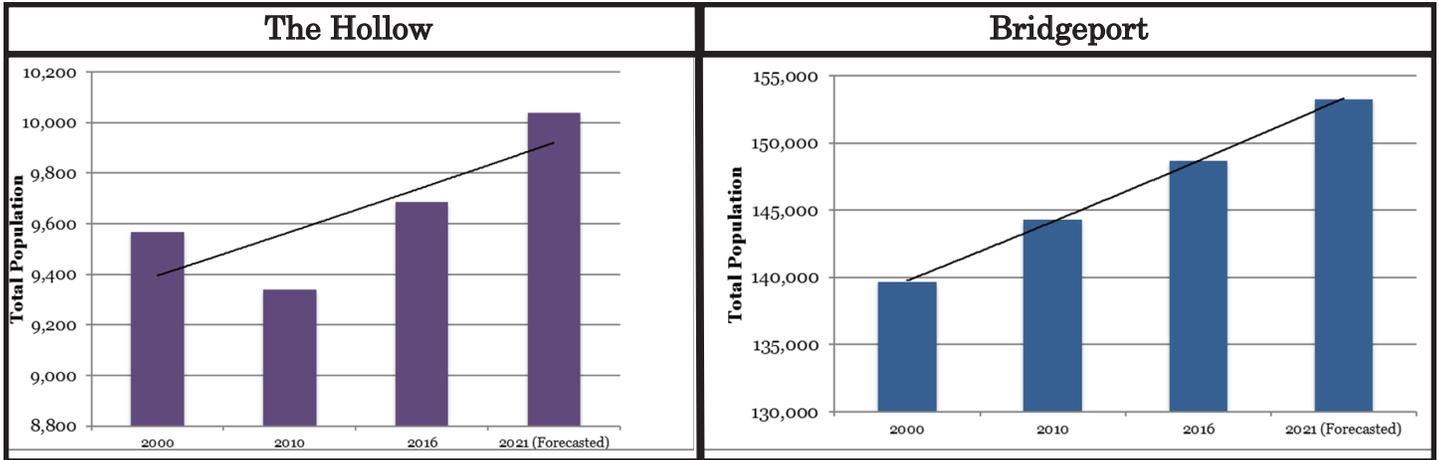
All demographic data was collected using ESRI Business Analyst, based on US Census Bureau data from the 2010 Census and data from the 2010-2014 American Community Survey. Below is the area defined in Business Analyst as the Hollow:



# 1. Population Demographics

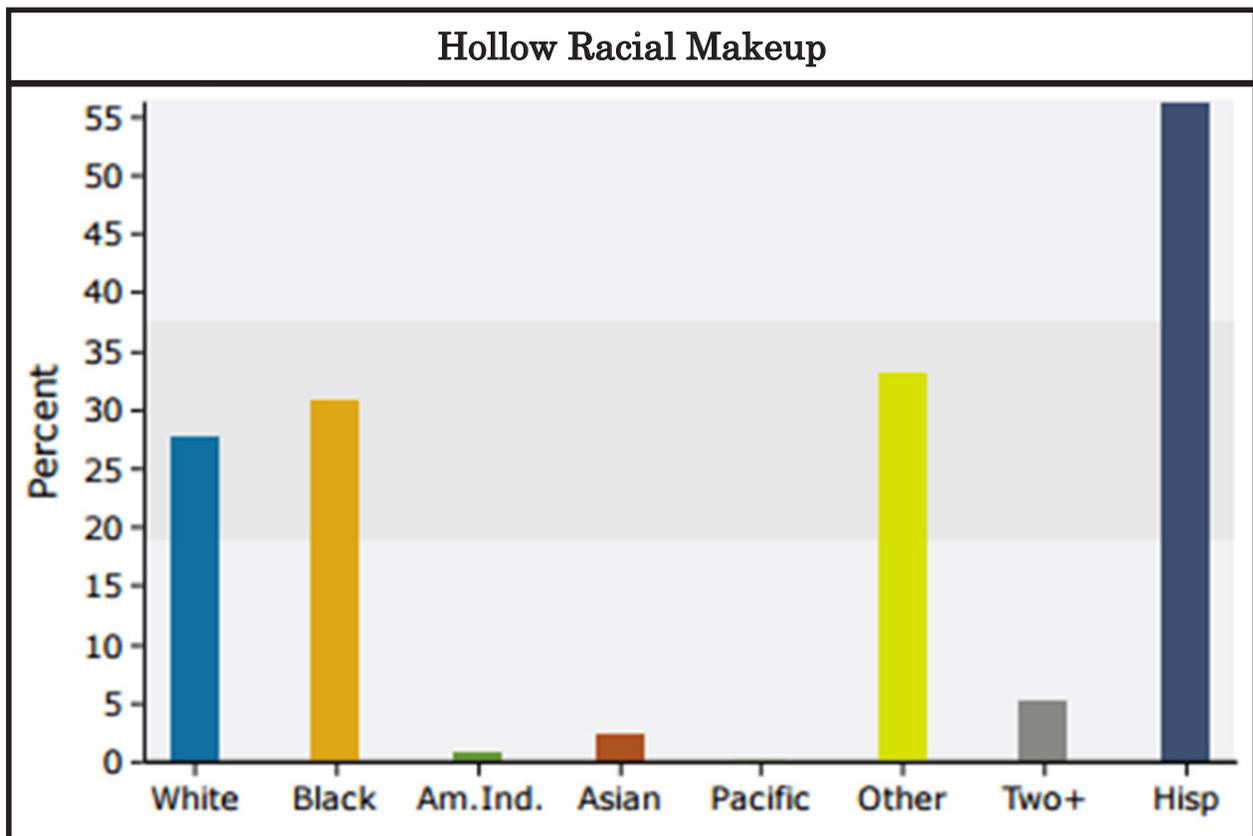
## Population and Growth:

The Hollow population in 2016 was 9,687 residents. The population is expected to grow at a rate of 0.72% per year from 2016 to 2021. This is greater than the City of Bridgeport's expected growth rate of 0.61%.



## Race and Sex:

The Hollow's population is 56% Hispanic, 31% Black, and 28% White. The Hispanic population is growing at a rate of 2.12% annually, while the Black and White populations are decreasing annually. 9% of Bridgeport's Hispanic population lives in the Hollow. The Hollow population is split evenly between men and women: 51% male and 49% female.



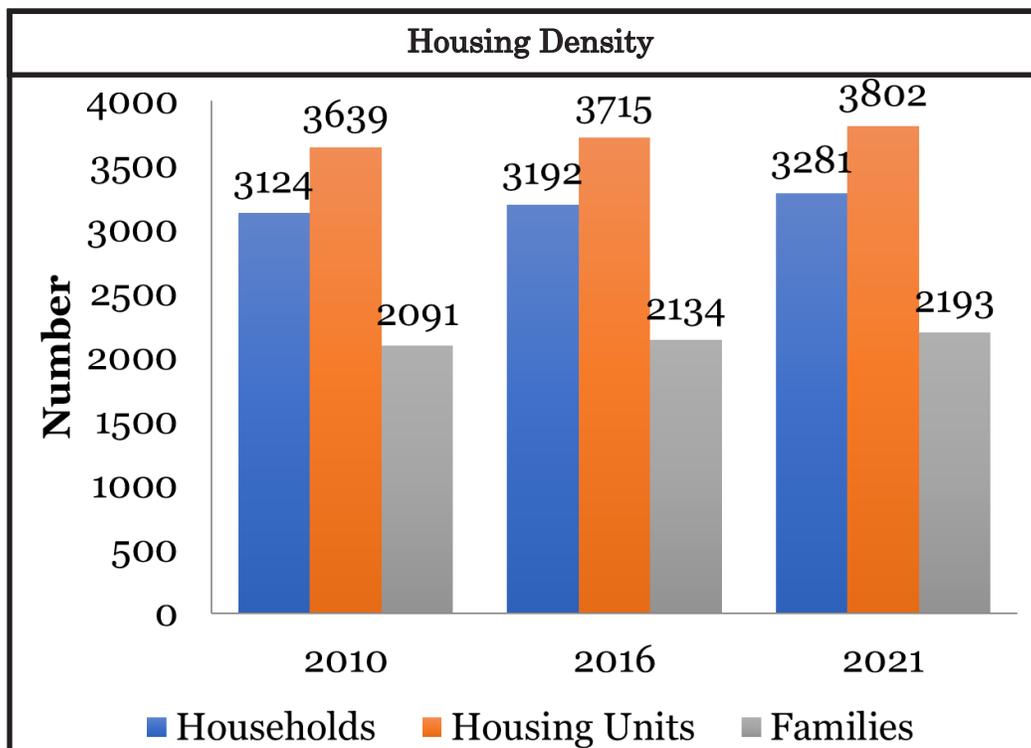
## Age:

The Hollow has a young population: 60.3% of the Hollow's population is under the age of 35.

Population by Age	Number	Percent
Age 0 - 4	897	9.3%
Age 5 - 9	853	8.8%
Age 10 - 14	732	7.6%
Age 15 - 19	717	7.4%
Age 20 - 24	837	8.6%
Age 25 - 34	1,805	18.6%
Age 35 - 44	1,350	13.9%
Age 45 - 54	1,110	11.5%
Age 55 - 64	763	7.9%
Age 65 - 74	393	4.1%
Age 75 - 84	162	1.7%
Age 85+	68	0.7%

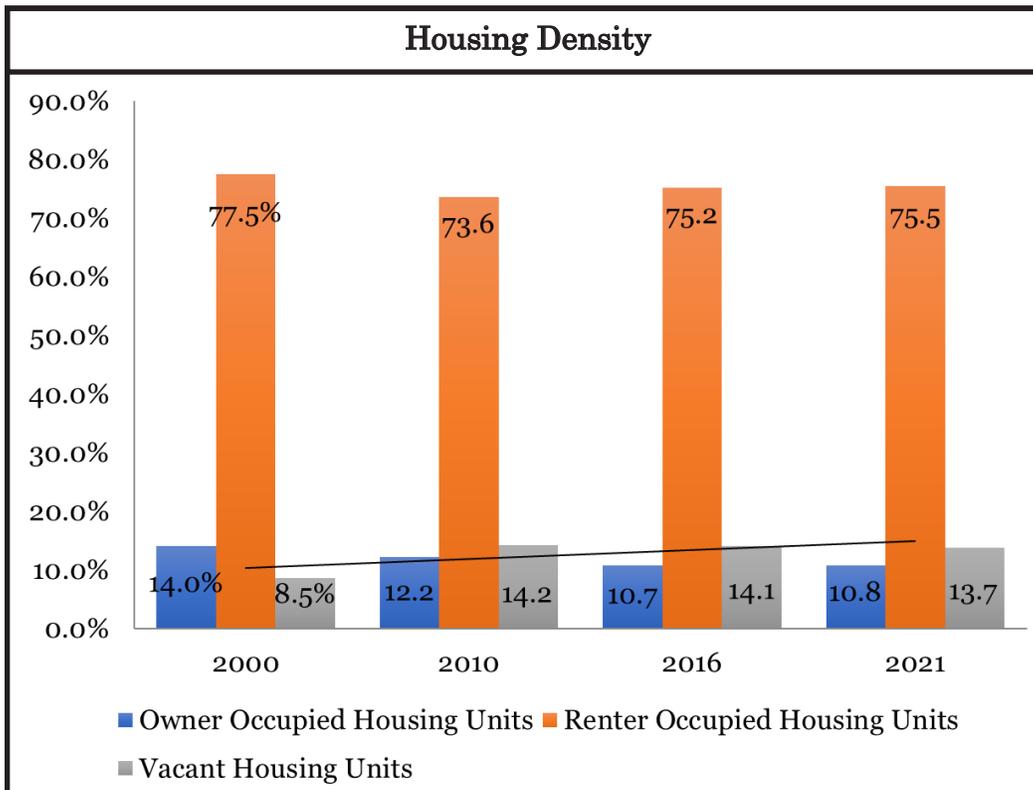
## 2. Households and Housing

There are 3,192 households in the Hollow, and 3,715 Housing Units. The average household size is 2.97 people. The growth rates of number of households, number of housing units, and household size are relatively equal over time.



Housing in the Hollow is primarily occupied by renters: in 2016, 75.2% of housing units were renter occupied.

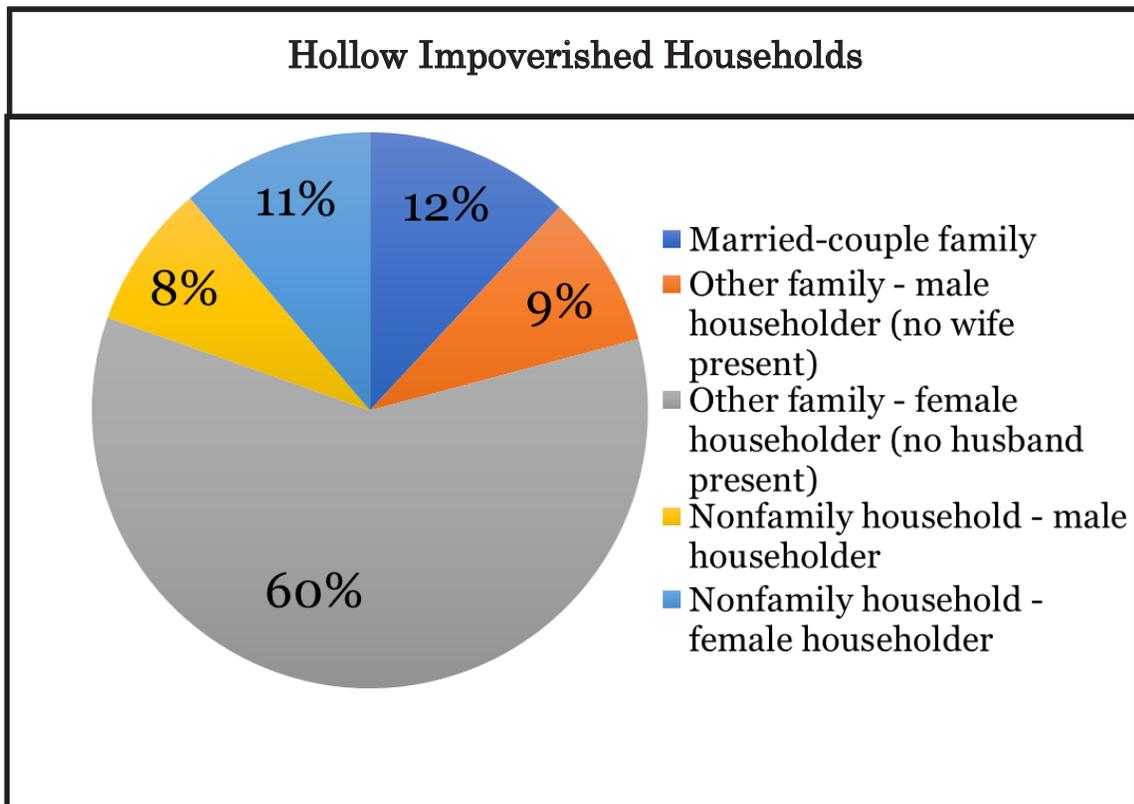
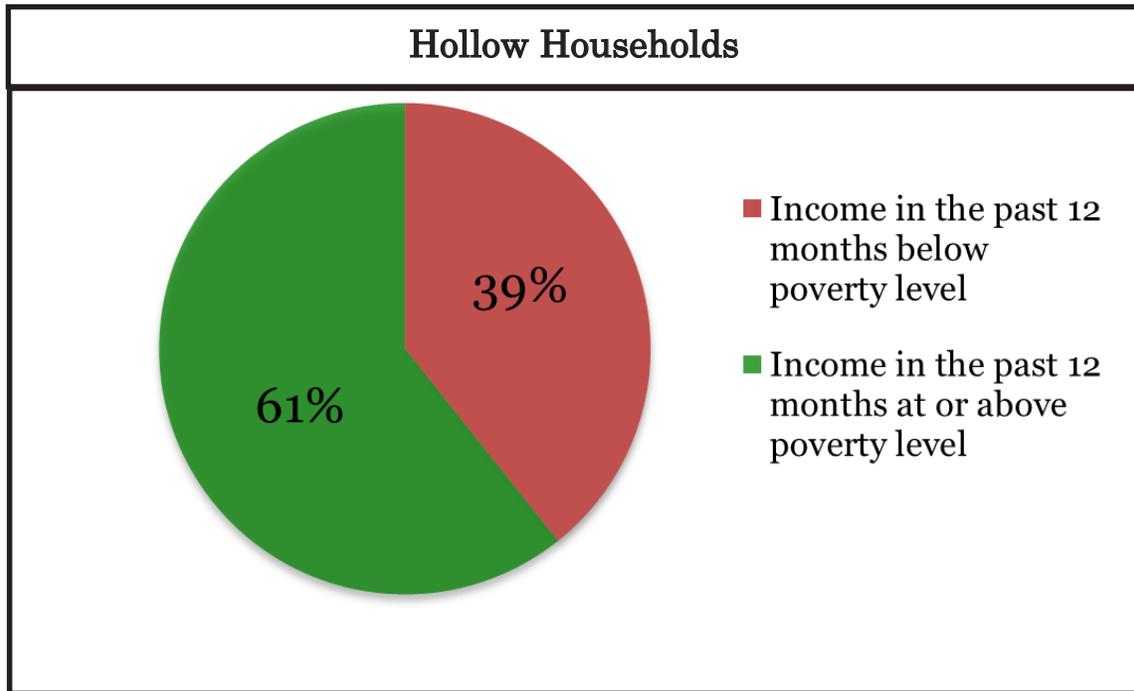
Vacant Housing: There are more vacant homes than owner occupied homes in the Hollow. The trend line below indicates that the number of vacant housing units is rising.



The Hollow has an older housing stock: 0% of housing units were built from 2000 to 2014. The median year housing structures were built is 1940. The median home value in 2016 was \$177,667

Built 2010 or later	0%
Built 2000 to 2009	0%
Built 1990 to 1999	1.1%
Built 1980 to 1989	4.5%
Built 1970 to 1979	7.8%
Built 1960 to 1969	12.8%
Built 1950 to 1959	11.4%
Built 1940 to 1949	11.3%
Built 1939 or earlier	51.1%

39% of the Hollow's households live below the poverty line. Of the impoverished households, 60% were single-parent households with a female householder.



### 3. Education and Employment

High School Graduates comprise the largest educational sector of the Hollow labor force. 63.3 % of Hollow residents have a High School diploma of higher. Most residents work in the service sector.

Employed Population by Industry (16+)		Educational Attainment of Population	
Agriculture/Mining	0.0%	Less than 9th Grade	16.5%
Construction	19.4%	9th - 12th Grade, No Diploma	20.2%
Manufacturing	9.5%	High School Graduate	29.1%
Wholesale Trade	2.5%	GED/Alternative Credential	5.0%
Retail Trade	14.2%	Some College, No Degree	15.6%
Transportation/Utilities	3.2%	Associate Degree	3.0%
Information	2.6%	Bachelor's Degree	6.3%
Finance/Insurance/Real Estate	3.3%	Graduate/Professional Degree	4.3%
Services	44.1%		
Public Administration	1.3%		
Total Number	4,061	Total Number	5,651

## B. Physical Development and Existing Conditions

### 1. Gateways to the NRZ

The NRZ neighborhood is bordered by three major roads: Washington Avenue to the east, Park Avenue to the south and North Avenue to the northwest. Existing transportation infrastructure, institutions with large land areas, and topography effectively limit the number of streets that enter the community. As a result, there are no roadways that actually pass through the entire neighborhood, but those that cross the perimeter roads lead to the center of the community. These roadways connect the NRZ to the rest of the city and as one crosses into the neighborhood there is a noticeable change to the street and block pattern, housing orientation and land use. Because the community edge is well defined, this shift from one community to another can be accentuated with subtle changes to signage, landscaping, street lighting and paving patterns to further distinguish the NRZ from the surrounding neighborhoods of Bridgeport.

# Gateways



Map indicates "gateways" that might be enhanced by signage and landscaping.

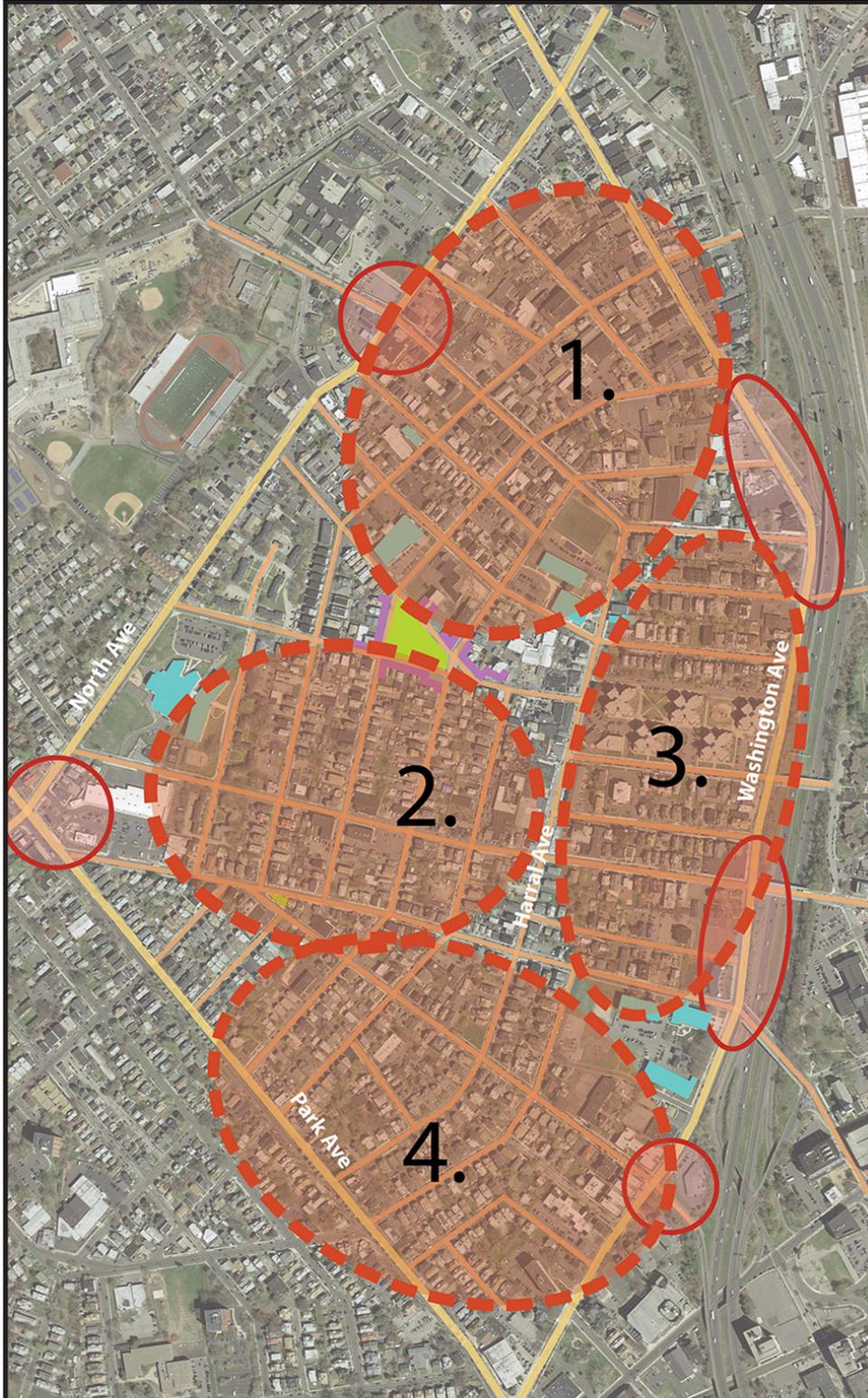
## 2. Lafayette / Nannygoat Park

Lafayette/Nannygoat Park is the apparent middle of the community. It is the only significant public green space in the neighborhood. Other community focal points include numerous schools and churches, that include their own associated open space, but as the largest public space located at the center of the community, Lafayette/Nannygoat Park is unequalled as a symbolic and physical community ‘town square’. Despite its prominence, the edges of the park are not well defined – there are vacant buildings along the northern boundary, and the architectural character is not much different from any particular side street in the area. By focusing on the edges, this area could be more established as the “center” of the community.



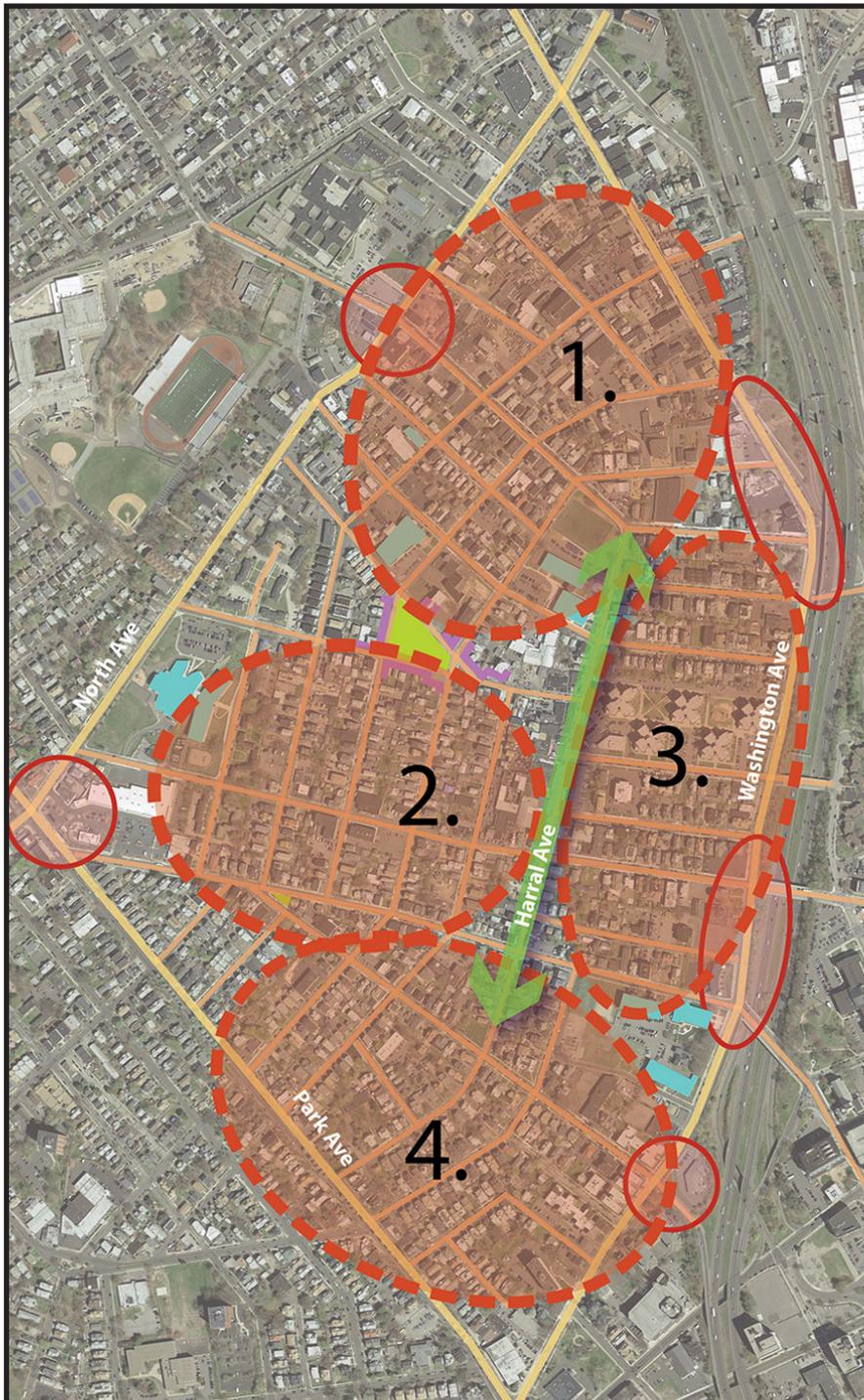
### 3. Community Sub-Districts

The Hollow NRZ area is an angled shape that is a result of the different streets, topography, and historical block patterns. These patterns yield markedly different characteristics that can be seen in the block lengths, frequency of intersections, solar orientation, minimal greenery, street continuity and view sheds. The angled street patterns yield four identifiable sub-districts that show different orientations and special attributes that distinguish these areas.



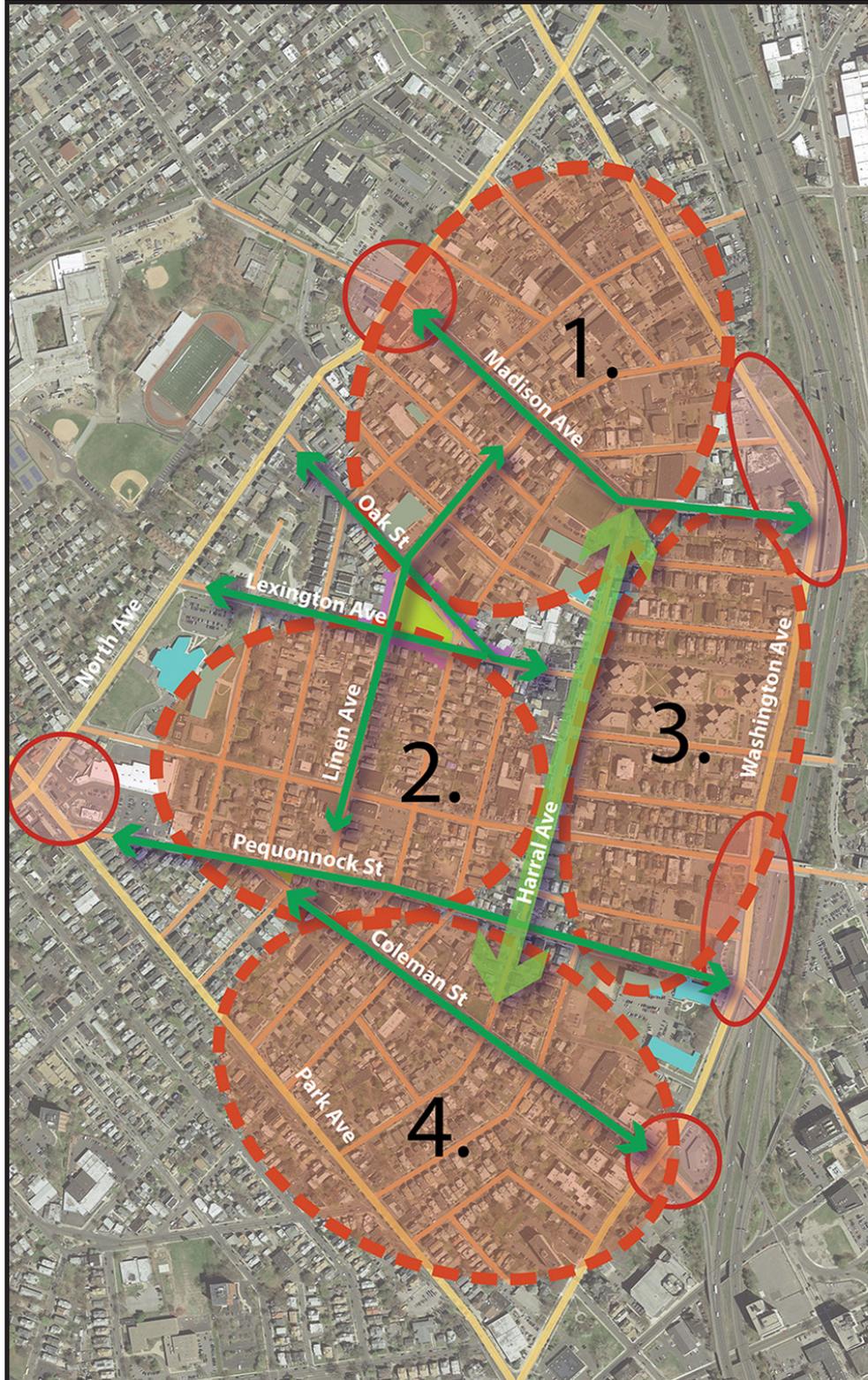
#### 4. Primary Corridor

Harral Avenue is one of the longest straight streets in the entire neighborhood and effectively connects all four sub-districts in the NRZ. It is also indirectly connected to nearby Lafayette Park at its midpoint via Lexington Avenue. It contains an unusual juxtaposition of land uses and a variety of building types that changes in character from block to block. The inconsistency of a recognizable urban design is a result of topographical changes; a mix of commercial, institutional and residential uses; and, the lack of a building and landscape similarity along its length. However, it's heavy daily use is truly a seam between the different sub-districts. In this way it emerges as the major locally used roadway, and as a mixed-use corridor at the middle of the NRZ.



## 5. Secondary Corridors

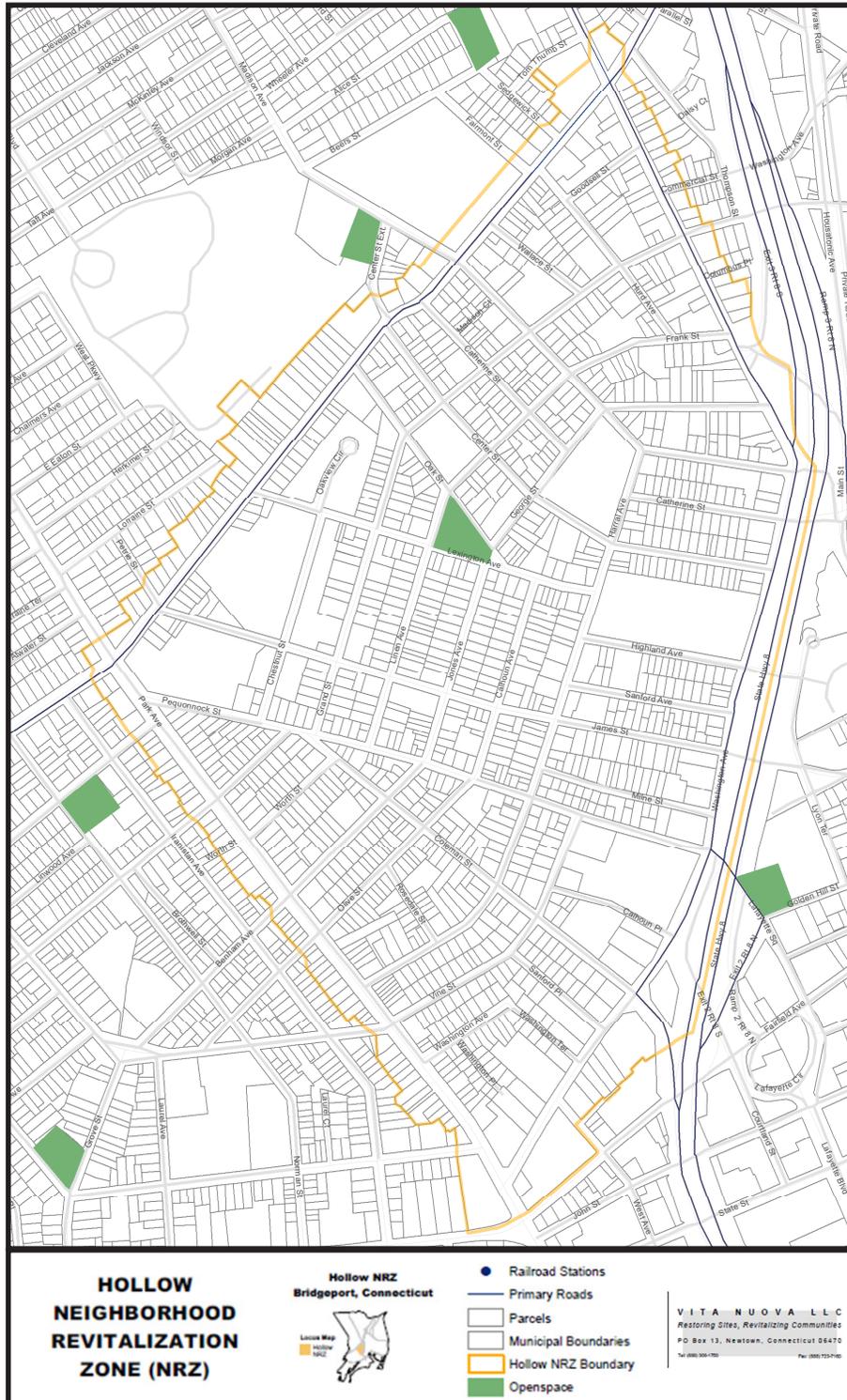
The roadways that lead to Lafayette Park, the NRZ gateways, Harral Avenue, and link the neighborhood together, generally run east-west. With the exception of Linen Avenue, which runs past the western edge of Lafayette Park, all other secondary roadways intersect with Harral Avenue. These secondary streets also suggest places for public realm improvements such as new street trees, special pedestrian lighting, and green infrastructure improvements.



## 6. Density and Open Space Measurements

With 9,687 residents in .42 square miles, the Hollow has the highest population density of all the neighborhoods in Bridgeport. In view of these numbers there is a notable lack of open space with only one park located in the center of the neighborhood. The map below shows a single triangular green space in the center of the Hollow: Lafayette Park. It is also known locally as Nanny Goat Park.

Coupled to the open Lafayette Park are the recently renovated Columbus Elementary School turf playfield and the adjacent splash pad that is open in the warmer months.



## 7. Stormwater Management

Because the overall topographic shape and elevation of the Hollow, there appears to be little or no flood risk. Having said that, the entire NRZ is dependent upon catch basins and municipal storm water lines. This may have been adequate for previous decades; however, that system was built with combined sewage and stormwater lines that, in times of heavy rain, surge and help overflow the capacity of the municipal waste treatment facility. The city is presently trying to fund as many system storm water improvement techniques as possible. One way for the Hollow to contribute to this effort would be to help support the installation of bio-swales and rain gardens where storm water is first filtered with native wetland plants, and allowed to infiltrate into the soil. In public areas, these devices take curbside drainage and let it drain through adjacent gardens. UCONN is one of the leading proponents of this work and offers a cell-phone app for designing rain gardens: <http://nemo.uconn.edu/tools/app/raingarden.htm>.

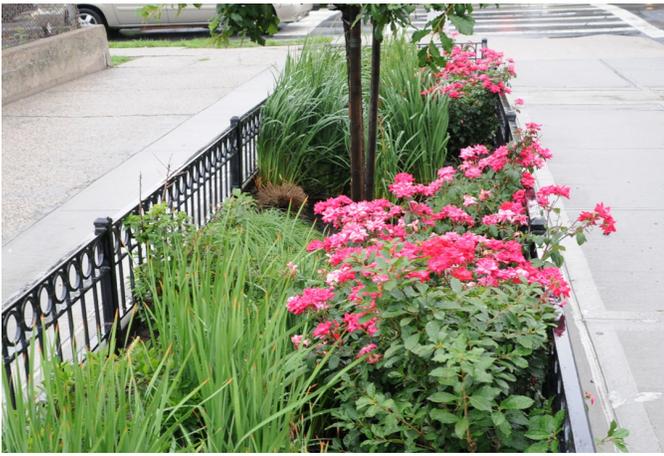


### A. Rain Gardens & Bio-swales

A simple statement of wanting environmental improvements in our neighborhoods is only the beginning statement of a long and involved process. We need to provide the opportunities at all levels of our community improvements to enhance the qualities of our neighborhoods. An awareness of environmental quality comes from understanding the extent the environment supports us. We need education given to our children where they can see little things that help their lives in the places that they live.

The Hollow has several restrictions on any number of community improvements: narrow sidewalks, little or no publically available land, old houses set very close to the sidewalk, a heavy demand for parking spaces, a large percentage of the community that is rental. In such a place as the Hollow, one very important piece of streetscape greenery that fits within its limits and can readily help improve stormwater runoff conditions is the use of rain gardens or more technically designed bio-swales.

Whereas the rain garden is made more to accommodate typical storm water coming off a building roof, the bio-swales are often made to help filter the water running down the street gutters. In the following five images we can see how these elements can contribute to the Hollow:



1) The potential for color along our streets with plantings that are a minimum of 3' wide.



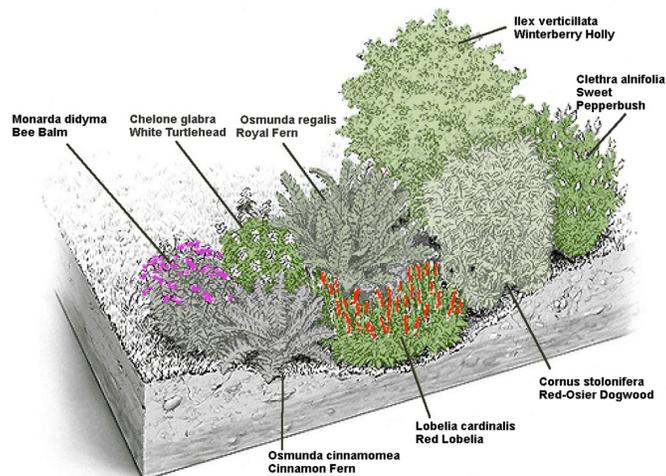
2) Here is shown the curb profile that lets gutter water detour into a planter where it can infiltrate or deposit pollutants and suspended solids (clay, sand, gravel, etc.).



3) This image shows a rain garden or bio-swale emptying water that is filtered during a heavy downpour.



4) A typical bio-swale for Bridgeport



A Typical rain garden or bio swale with Red Lobelia & Royal Fern occupying the lowest, & wettest zone

5) A sketch showing plants that could easily grow throughout the Bridgeport.

## C. Municipal Infrastructure

1. Perhaps the most obvious examples of infrastructure are the ones we use every day, but yet don't think about. Part of the reason we don't see them is because they aren't moving, but they frequently help people move. Our streets are the biggest single infrastructure component and they help support what's above and below them. Electrical lines held high above the street and the gas lines buried beneath them are inaccessible for safety, but bring us our energy for heating and lighting; drinking water, sewers, and storm water lines are also buried to protect them, but also to allow gravity to help collect our waste or unwanted water. Future forms of energy that come from solar or wind energy are beginning to change not only how we think of our communities, but also how they affect them. Utility poles, catch basins, man holes, and street lights are only some of our infrastructure elements that are now being augmented with solar panels, wind turbines, and rain gardens. In the next generation even the Hollow will change it's character as both environmental and economic pressures help us change our environment.

### 2. Streetscape & Parking

The main parking accommodation in the Hollow is parking along both sides of the street. There are a few private businesses that store or repair cars that are parked on private land, but in general, there is a shortage of available parking for residents. Coupled with the minimal parking is the absence of street trees, and that many sidewalks and curbs are damaged from renegade and illegal parking.

In the study of the Hollow, the NRZ team, lead by PRE/view Landscape Architects, looked at parking alternatives that examined alternate side parking, "on and off street" parking, as well as resident permit-only parking. The review, comments, and possible layouts offer the city and residents more questions but also offer a possible solution for the Hollow. The following is a summary of the 3 possible alternatives for added parking.

<b>Studied Parking Alternatives</b>	<b>Pros</b>	<b>Cons</b>
1. <b>Resident Permit-Only Parking</b>	Limits on street vehicles	Bureaucratic operation Expense to operate Expensive to enforce A hassle for residents, visiting guests, etc. Service vehicle access
2. <b>Off Street Parking</b>	Adds more spaces for residents	Purchase of private land Expense to monitor users Added Costs to users for maintenance Potential security risk

(Studied Parking, cont.)

3. On Street Parking

Adds more spaces for residents  
No land to purchase  
No added annual Costs  
Faster Police/Fire Access & Visibility  
Easier Snow Removal  
Easier Street Cleaning  
No Daily Changes  
No Annual Permits  
More Business Parking  
Closer House Parking

Backing into traffic  
Roads may not be wide enough in all areas  
Service Vehicle Access versus side of street

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Parking on Linen Street

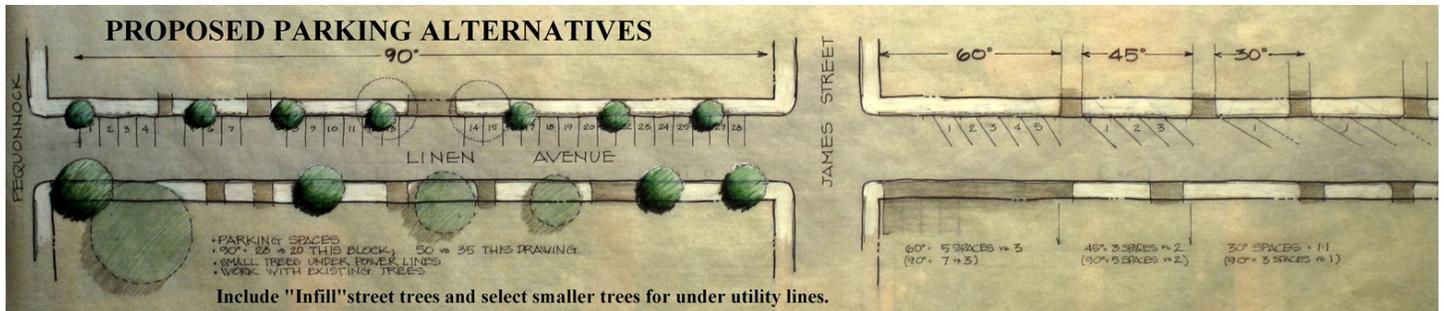
Existing

Upon reviewing the alternatives it was decided that a likely alternative, would include minimal cost to the city as well as residents of the Hollow.



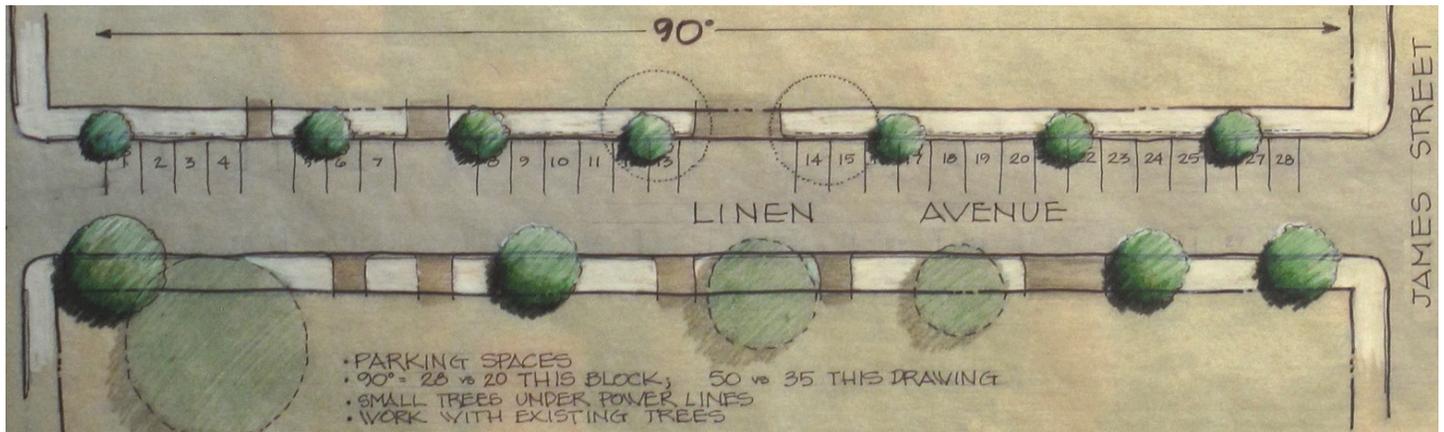
This is the existing theoretical maximum parking (35 spaces) in the area shown; however, fire hydrants, setbacks, etc. reduce actual parking quantities.

Potential



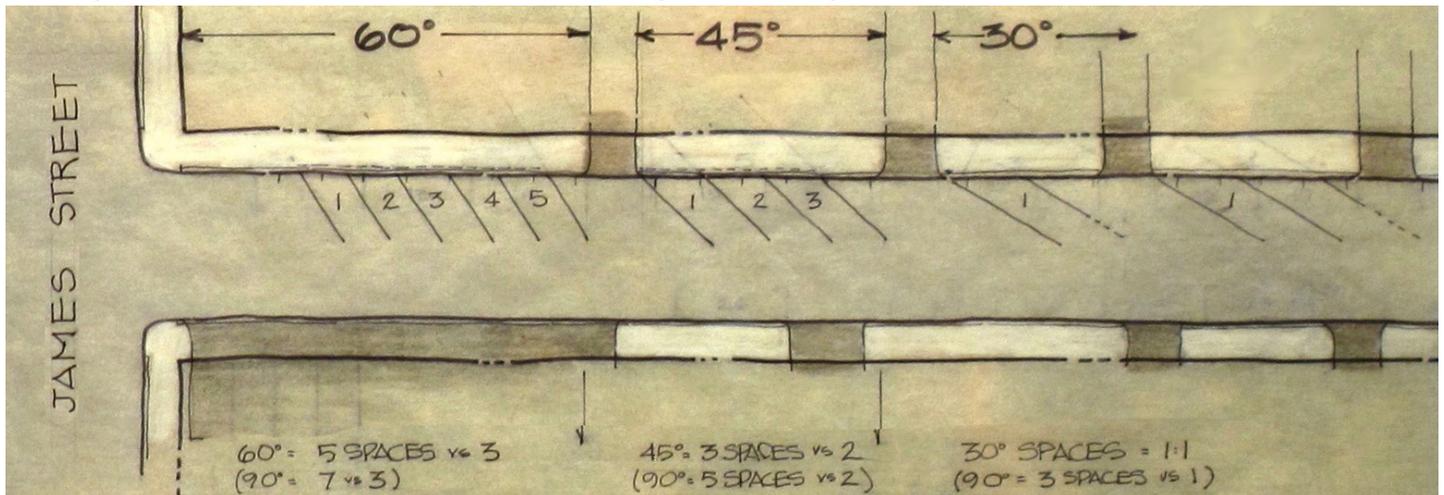
This example of possible parking alignments shows that 90° angle could accommodate as many as 50 cars in approximately the same space. The other angled alignments offer fewer spaces and actually more restrictions on use (e.g., single preferred approach direction). Also note that planting trees in the roadway (the planting space between a curb and walkway) will impact the available space for parking.

### Enlarged Detail (Possible) 90° Parking



In this drawing, the smaller trees (under power lines) are shown placed at the measured position between two spaces in order to accommodate the vehicle overhang, However, this still leaves them vulnerable to being hit, damaged, and eventually killed.

### Enlarged Detail (Possible) 60°, 45° & 30° Angled Parking



Although no trees are shown in this portion of the studied plan, the trees in angled parking areas may have a better chance of surviving because when the passenger-side tires hit a restraint, it prevents an increasing amount of the vehicle from overhanging the curb and accidentally hitting a tree.

## PARKING CALCULATIONS

(BASED ON FIELD ESTIMATES)

LINEAR FEET OF STREETS WITHIN HOLLOW	50,000 LF
EXCLUDE BOUNDARY STREETS	>13,900 LF
EXCLUDE MINOR STREETS	> 5900 LF
STUDIED ROADWAY (CAD BASED ZONING MAP)	30,900 LF (One Side)
DRIVEWAY OCCURRENCE & SETBACKS*	-20% to 25%
RESTRICTED NO PARKING (HYDRANTS, ETC)*	-5%
INTERSECTIONS & SETBACKS*	-10%
DIMENSIONS WHERE FULL SPACE MAY NOT FIT**	-5%
STUDIED ROADWAY MINUS LOSSES	17,000 LF

# HOW MANY VEHICLES CAN FIT IN 17,000 LF

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## EXISTING PARKING SPACES

ESTM. PARALLEL PARKING @ 23'-0"=	750 CARS
90° PARKING IN STUDIED AREA	
ESTM. RIGHT ANGLE PARKING @ 9'-0"=	1889 CARS
60° PARKING IN STUDIED AREA	
ANGLE PARKING @ 80% OF 90° SPACES	1510 CARS
45° PARKING IN STUDIED AREA	
ANGLE PARKING @ 60% OF 90° SPACES	1130 CARS
30° PARKING IN STUDIED AREA	
ANGLE PARKING @ 50% OF 90° SPACES	945 CARS

Parking Calculations provided by PRE/view Landscape Architects

\* All estimates, made by PRE/view Landscape Architect, are based on field observations, 20 JAN 2017.

\*\* Because vehicles are either parking on an angle, or the increment of each full parking-stall-width totals less than the available curb lengths between restrictions (driveways, fire hydrants, etc.), the cumulative 17,000 LF will need to be adjusted with actual field measurements. Lastly, the varying angles are shown with an approximate percent of "vehicles in an equal area of 90 degree parking."

## D. Neighborhood Physical Characteristics and Assets

### 1. Landscape and Structures



*This attractive stone house sits hidden in plain sight on Pequonnock Street between Harral Avenue & Olive Street and appears to be approximately 100+ years old. In keeping with the skillful masonry building material are a number of carefully built tapered top-stone fence pillars that are topped with pebble & shell encrusted mailbox-like planters.*

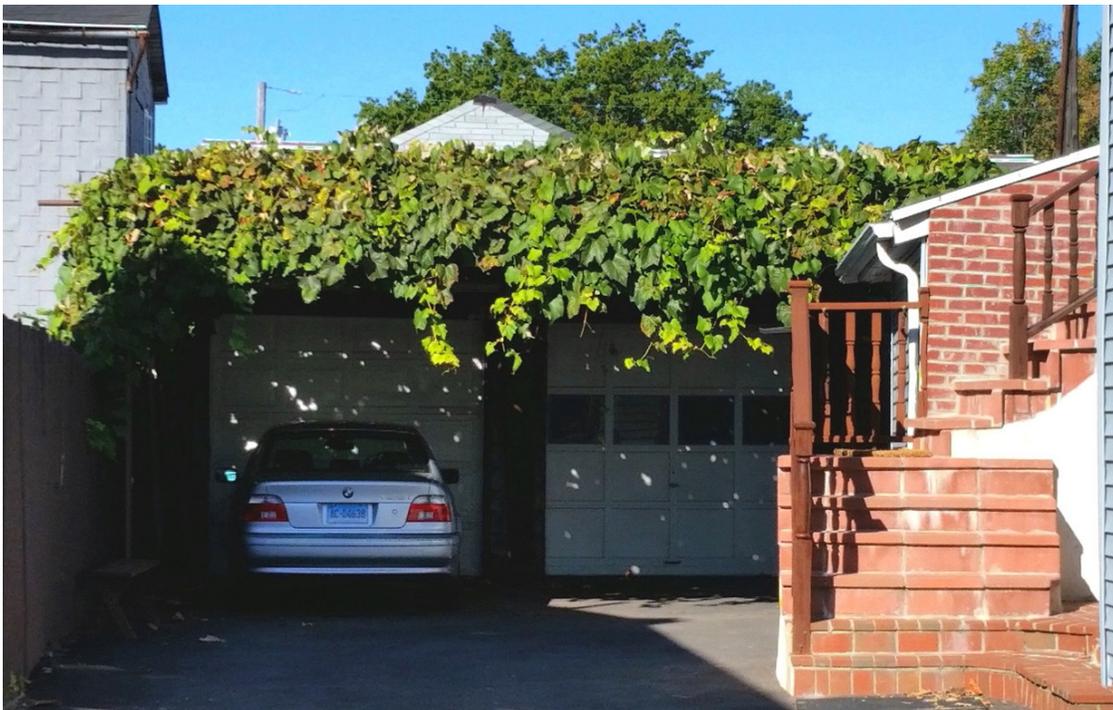
## Historical Character

i. Perhaps a simplified description of the area would define the Hollow as in transition from 19th & early 20th Century low and moderate income housing to lower income housing with opportunistic start up businesses that, for the most part, provide local services. There are 2 or 3 construction or manufacturing companies that provide both local and regional sales or service, but these are atypical and show signs of declining businesses. Coupled to the use-changes is the fact that there are far fewer trees remaining in this area than the previous decades. Whether it was urban pollution or the residents wanting to park more vehicles closer to their house (convenience and security), the removal of trees is one of the major life quality concerns for this area.

Accompanying the small typically single lot business are makeshift building additions and high chain link fences (many with barbed wire or razor tape). Whereas the small businesses (frequently automobile repair) are mixed in among converted multi-family residential buildings they provide some jobs and economic aid to the community. We suspect that the area will continue to see an increase in larger multifamily housing units as well as the existing businesses growing and providing more employment. The downside to the commercial growth is that many sites appear to have un-policed zoning violations (outside equipment and material storage, signs, etc.).

### ii. Uniqueness & Private Site Improvements

Part of the charm of the Hollow lies in finding historical elements that are indicative of the previous residents in this area of Bridgeport. Throughout the area are grape arbors that were planted by Italian and Portuguese residents. Before them were Irish workers that added stone and concrete details. Some are set with beach stones and shells. These details are now guarded and used by newer immigrants as they too see this area of Bridgeport as a stepping stone for their future.



*With the houses and garages often relatively close to the streets, so too are many of the arbors. They are a welcome addition of green to a hard urban space and frequently provide summer shade & protection for automobiles.*



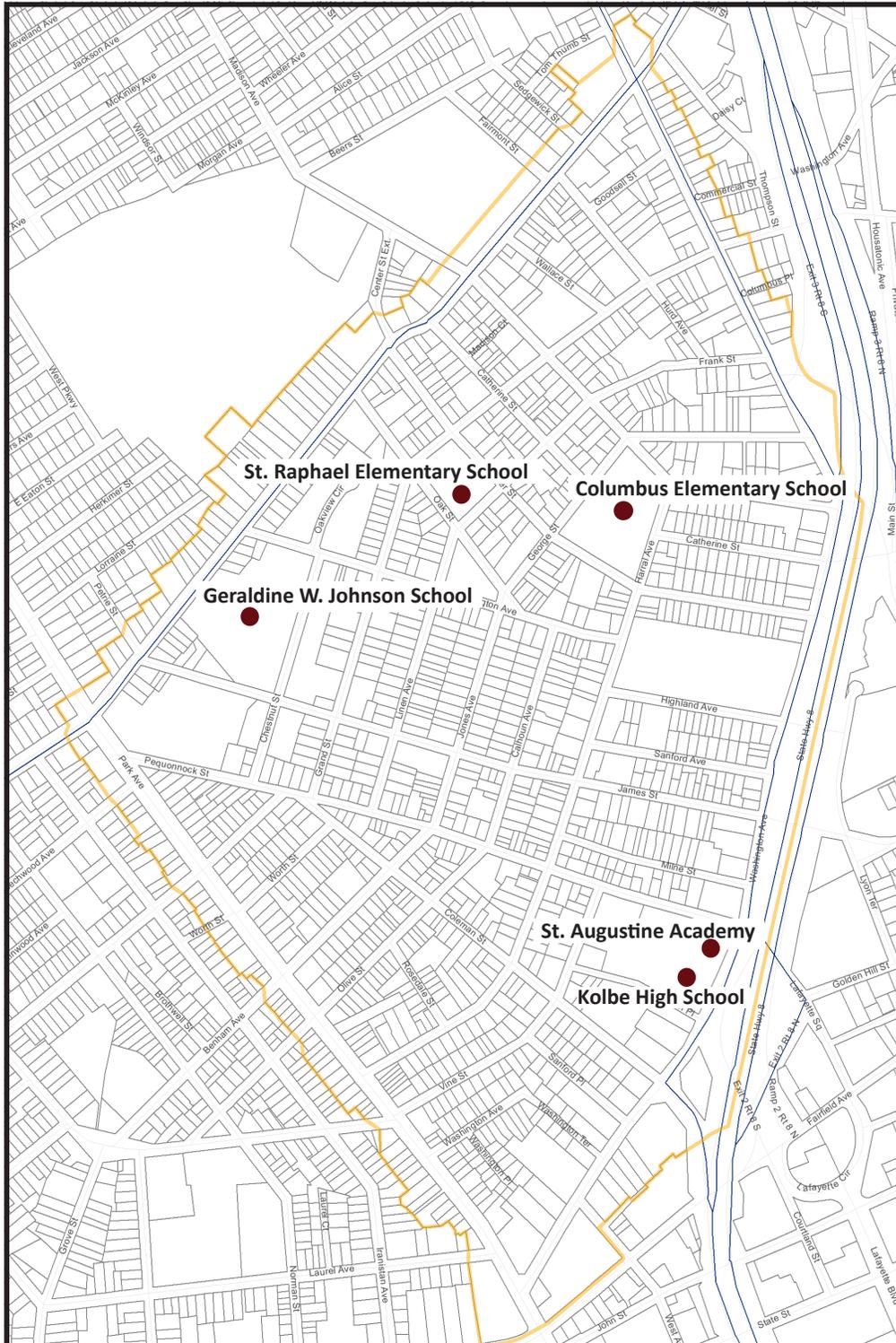
*Backyards, although often used as parking space, are also provided a shade opportunity as well as frequently providing a large amount of table & wine grapes.*



*About ten years ago, the excavation for a house in BPT hit a buried foundation of a 100 year old structure that had this same twisted steel rebar for reinforcing. Perhaps at the same time it was installed for fences to protect the front yards of two large multifamily houses on Madison Avenue between Frank and George Streets. Today this small section is helping protect a peach tree.*

## 2. Schools

In general, the Hollow has two categories of schools: public (Columbus Elementary School, Geraldine Johnson Elementary School) and parochial (St. Augustine Academy - elementary, Kolbe High School). However, within a couple blocks of the Hollow NRZ is Central High School – one of the largest public high schools in Bridgeport. As a separate note, it appears the Diocese of Bridgeport may be closing St Raphael church. It is not clear if the school will also be closed. The school is presently part of the Diocesan Academy system and houses pre-K through 3rd grade students with a Catholic-based education of literacy, science, and mathematics.



### 3. Religious Facilities

A lot can be inferred from the number and size of religious facilities that exist in the Hollow. If you stood at the southernmost tip of Hollow NRZ area, near the intersection of Fairfield and Park Avenues, within about 1 block you can see 6 religious buildings. Of the 6 buildings, perhaps the largest building is the gray granite St. John's Episcopal Church. Looking south from St. Johns and within one block are the front steps of a brick church - the former United Congregational Church. It was recently sold to the Islamic Community in Bridgeport for conversion to a mosque. To the east of St. John's about one block is the brownstone First Baptist Church that Google shows as the home of Hope Home Care, LLC.

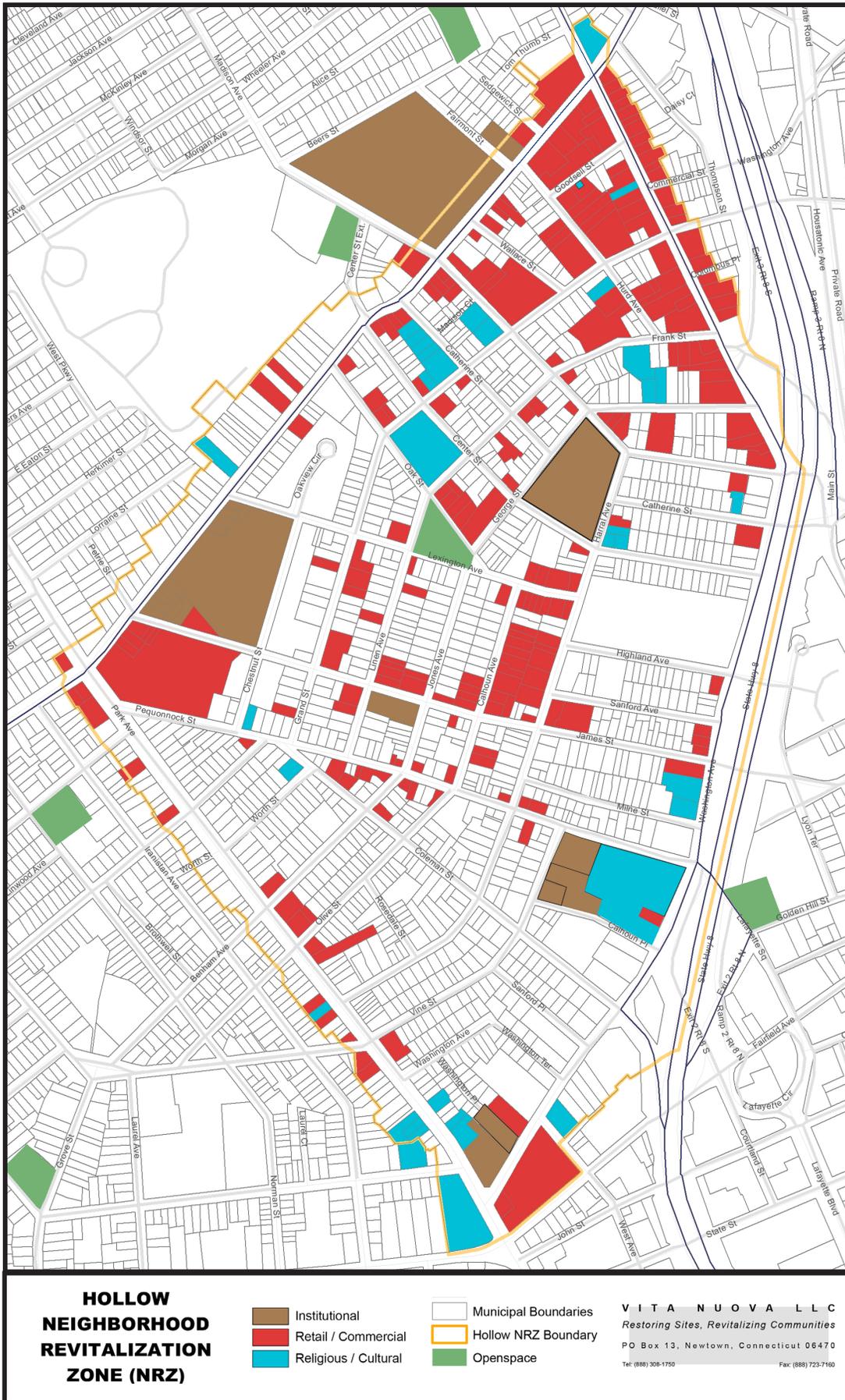
A block north of St. John's on Park Avenue is the home of New Hope Missionary Baptist Church. The building itself is now a mix of brick, stucco, polished stone and concrete. At the peak of the roof are tablets of the 10 Commandments with Hebrew inscription. Further north and within the New Hope Missionary Baptist Church block is a storefront church: Oasis de Vida Centro Christiana. It is partially covered with a banner asking if you "Need a Miracle?" Lastly, the cross street that borders to the north of St. John's is Laurel Avenue. Within that first block is the cream yellow brick & stucco church: Inglesia Pentacostal Primera Casa de Oracion.

These facilities speak quietly of the influx and change of community members that are no longer here. The community is changing such that the recent wave of immigrants are often not primarily English speaking residents, nor are some Christian. These religious facilities exist within a few blocks of the Roman Catholic, St. Augustine Cathedral, elementary school, Kolbe Cathedral High School, and the outdoor Divine Mercy Shrine.

Clearly religious facilities are numerically the largest single group of cultural facilities in the Hollow. It is recommended therefore, that an effort be made to reach out to all of the facility leaders and try to develop an active set of community links that help to organize activities, requests for municipal services, and greater inter-faith friendships in their neighborhood.



## 4. Land Use



## Land Use - Commercial & Retail Activity in the Hollow

A general land use study was conducted in the Hollow and many changes are underfoot. In addition to increases in multi-family and apparent density, commercial/retail establishments are continuing to spread throughout the neighborhood as this map suggests. While no quantitative survey was conducted, and while traditional businesses continue to operate, many newer businesses, some oriented toward new immigrants, are operating throughout the neighborhood. The northeast corner of the Hollow continues as a solidly-commercial area, but commercial/retail can be found on most corners of the Hollow. Behind multi-family residential and commercial/retail, religious or faith-based institutions are the next largest category of land uses. Public open space continues to be a need in the Hollow.

Touring the neighborhood shows the diversity of land uses within the Hollow. The general categories fall into 4 major groups (in somewhat of a rank order although not specifically counted) and a minor last one (although its impact may be more important than known):

1. Construction services and contractor storage facilities along with their labors' housing and vehicle storage;
2. Motor vehicle sales and services including towing, body repair, and gas stations that probably employ the second most number of area residents;
3. Food and consumable sales including restaurants, convenience stores, cultural organizations, taverns, and package stores;
4. Supplies and sales of fabric, paper, plumbing , paint, and glass products;
5. Financial services including banking, check cashing, loan and pawn shops. After excluding the bank, it becomes clear that the area is supported with businesses that may or may not take advantage of the labor pool, however, the typical higher interest rates and marginal loans or sales (pawn shops) indicates that many of the local residents need or depend on expensive financial services that have an effect of draining some of the economic well being of the community.

Perhaps the most significant fact of the commercial and retail businesses in the Hollow is that although the majority of the area is zoned residential (both B & C), it appears that at least 35% of that area has zoning that has been either issued use variances or is a grandfathered operation within the Hollow area. In any typical residential area in Bridgeport this might become a land use issue, but this fact has created a vibrant setting where new residents live, work and obtain services.

## 5. Vacant and Underutilized Property

The consulting team conducted a survey of the neighborhood and identified 30 vacant and/or underutilized properties in the Hollow. Given the density of development, this is a large number and should be addressed as part of further planning work in the Hollow. Some can be used to alleviate part of the parking issue and others should be used to ensure vacant space is productively used.



There are a few significant underutilized buildings including the Ostermoor site across from Lafayette Park, also known as Nanny Goat Park.

All vacant and underutilized properties listed below should be performing at their highest and best use. Accomplishing this entails working with the property owners to communicate the NRZ vision; providing assistance in connecting the property owner with developers or entities which will purchase and/or develop the property in accordance with the NRZ Plan; or acquisition of properties by the City.

### Details of The Hollow's Vacant and Underutilized Properties

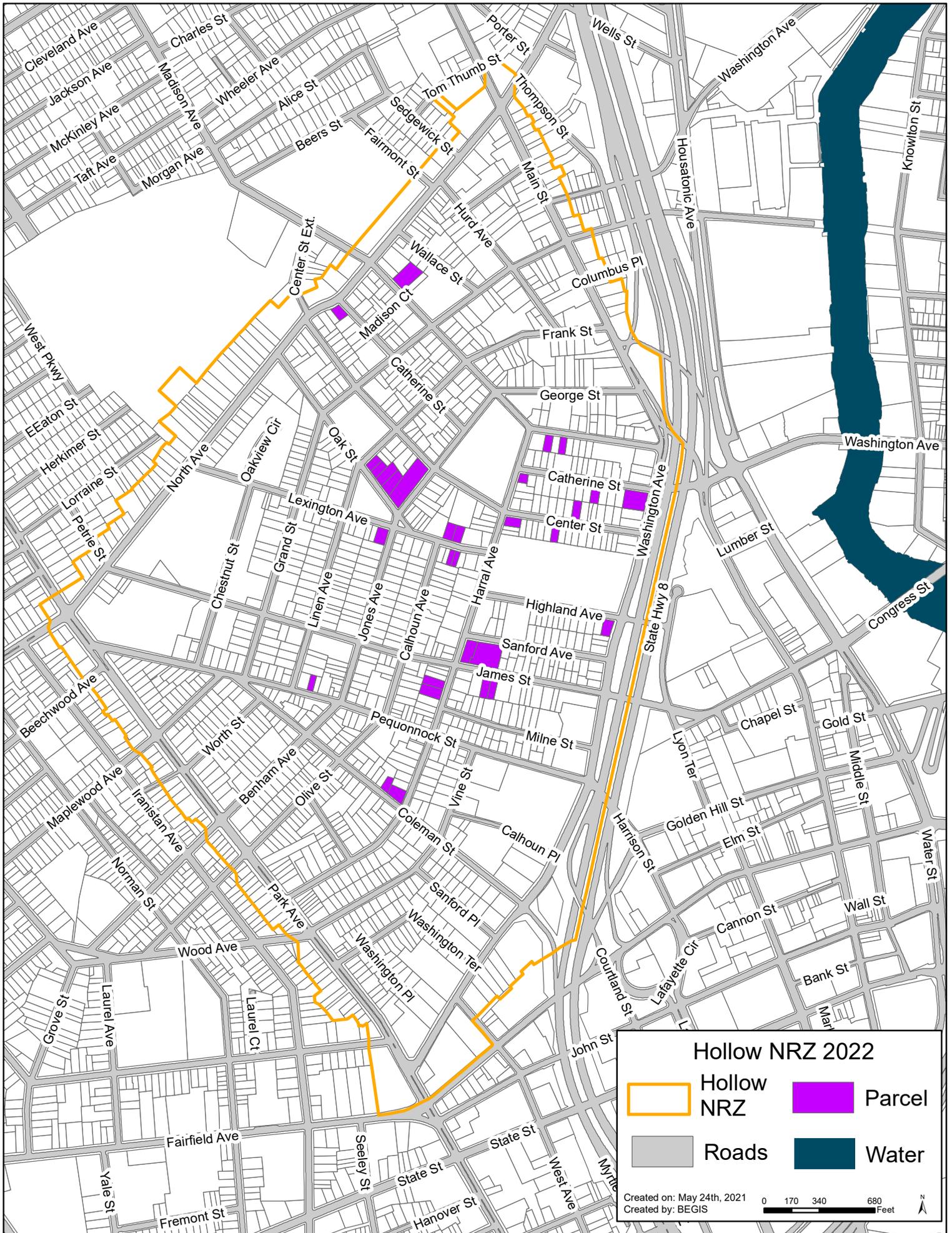
	ADDRESS	UNIT	PARCEL ID	OWNER OF RECORD	OWNER'S ADDRESS	COMMENTS	SIZE (ACRES)
1	394 Madison Avenue	#396	1003-2	Pembroke Laundry & Cleaners	396 Madison Ave, Bridgeport, CT 06604	Unoccupied commercial building	0.26
2	1249 North Avenue	#1255	1005-13A	Pedreira Albertina	1265 North Ave, Bridgeport, CT 06604	Vacant area. Used as parking.	0.14
3	82 Oak Street	#86	1025-1	82 Oak Street LLC	592 Fifth Avenue, New York, NY 10036	Vacant area with vacant garage and outbuildings.	0.11
4	4 Oak Street	N/A	1025-2	82 Oak Street LLC c/o CT Century Gardens LLC	12 East 49th Street, 39th Fl, New York, NY 10017	Unoccupied warehouse.	0.2
5	60 Oak Street	#64	1025-3	82 Oak Street LLC	592 Fifth Avenue, New York, NY 10036	Unoccupied outbuildings and land	0.4
6	304 George Street	N/A	1025-4	Wood Oak Apartments LLC c/o CT Century Garden LLC	12 East 49th Street, 39th Fl, New York, NY 10017	Unoccupied warehouse	0.67

(continued on next page)

## Details of The Hollow's Vacant and Underutilized Properties (continued)

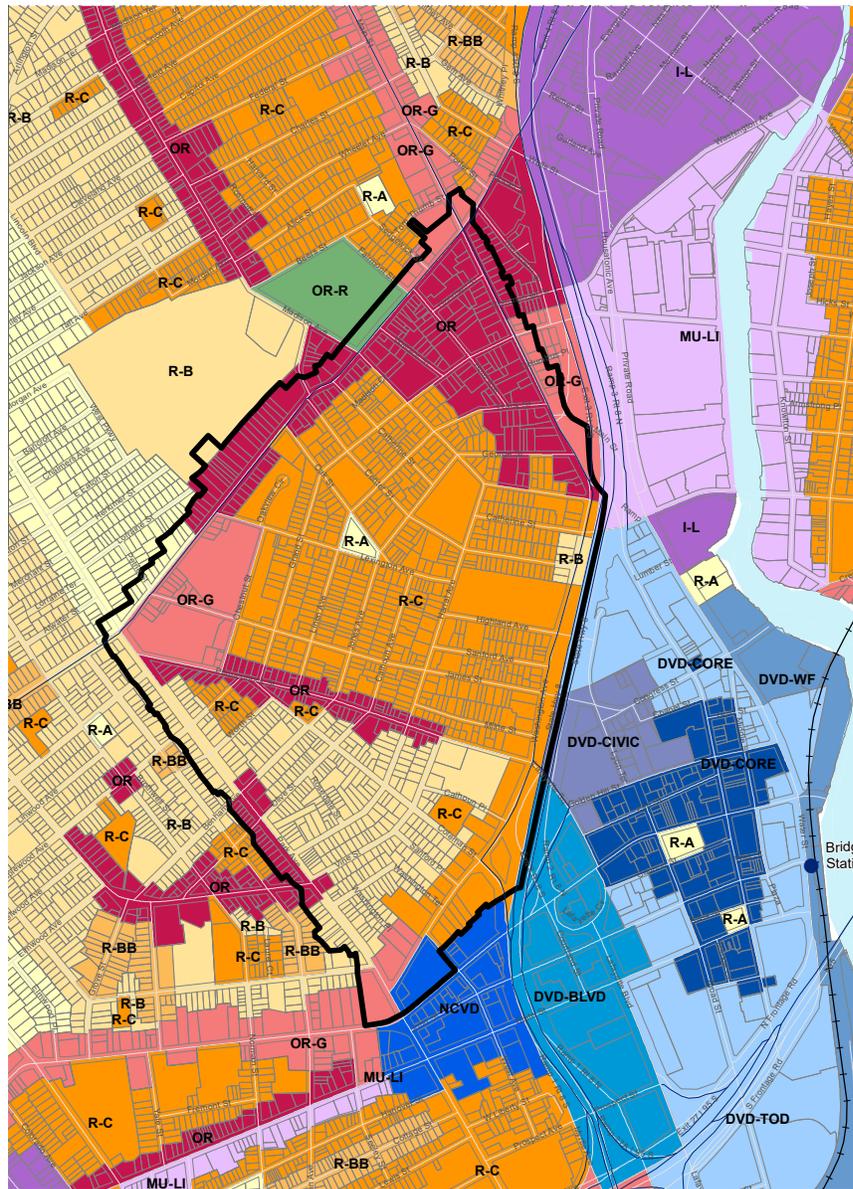
	ADDRESS	UNIT	PARCEL ID	OWNER OF RECORD	OWNER'S ADDRESS	COMMENTS	SIZE (ACRES)
7	92 Oak Street	#96	1025-11	82 Oak Street LLC	592 Fifth Avenue, New York, NY 10036	Vacant area	0.11
8	203 Jones Avenue	#209	1027-27	Esteves Maria Et Al	203 Jones Ave, Bridgeport, CT 06604	Lot area (near Lexington) is underutilized and used for Parking	0.15
9	354 Pequonnock Street	#356	1028-7	Alves Ricardo Et Al	215 Country Hill Drive, West Haven, CT 06516	Vacant area used as parking lot	0.08
10	200 Coleman Street	N/A	1034-7	Park City Housing & Development Corporation	Exempt Parcel N/A Bridgeport, CT	Vacant area	0.1
11	191 Harral Avenue	#197	1034-8	Housing Authority of the City of Bridgeport	150 Highland Ave, Bridgeport, CT 06604	Vacant area	0.16
12	323 Harral Avenue	N/A	1037-14	Housing Authority of the City of Bridgeport	150 Highland Ave, Bridgeport, CT 06604	Vacant area enclosed by fence	0.21
13	339 Harral Avenue	N/A	1037-15	Pro Tech Homes LLC	640 Shelton Rd, Trumbull, CT 06611	Vacant area enclosed by fence	0.15
14	211 Lexington Avenue	#213	1038-21	Costa Antonio	189 Lexington Ave, Bridgeport, CT 06604	Vacant area used as parking lot	0.08
15	217 Lexington Avenue	#219	1038-22	Costa Antonio	189 Lexington Ave, Bridgeport, CT 06604	Vacant area used as parking lot	0.06
16	230 Lexington Avenue	#236	1040-7	Paniccia Maria	29 Essex Lane, Trumbull, CT 06611	Vacant area used as contractor yard	0.11
17	222 Lexington Avenue	N/A	1040-8A	Paniccia Americo	36 Essex Lane, Trumbull, CT 06611	Garage and area used as contractor yard	0.16
18	115 Madison Avenue	N/A	1045-20	Criande Properties LLC	179 William St, Bridgeport, CT 06608	Vacant area used as contractor parking	0.09
19	135 Madison Avenue	#141	1045-21B	WCG12 LLC	97 Catherine St, 2 <sup>nd</sup> Fl, Bridgeport, CT 06604	Unoccupied 6-family building	0.11
20	207 Catherine Street	N/A	1046-1	Housing Authority of the City of Bridgeport	150 Highland Ave, Bridgeport, CT 06604	Vacant area enclosed by fence	0.08
21	775 Washington Avenue	N/A	1046-14	Oppedisano David	26 Frost Hill Road, Trumbull, CT 06611	Unoccupied warehouse building with vacant area	0.37
22	117 Catherine Street	#119	1046-18	Carrena Luis	24 Tuckahoe Road, Easton, CT 06612	Vacant area used as parking lot	0.13
23	84 Center Street	N/A	1046-7A	Cabezas Washington	1440 Madison Ave, Bridgeport, CT 06606	Vacant area	0.13
24	552 Harral Avenue	#560	1047-1	Almonte Luis	1217 Iranistan Ave, Bridgeport, CT 06604	Vacant area used as parking lot. Former community garden	0.13
25	103 Center Street	#105	1047-31	Housing Authority of the City of Bridgeport	150 Highland Ave, Bridgeport, CT 06604	Vacant area	0.11
26	621 Washington Avenue	N/A	1049-27	NYCT Management LLC	40 Southport Ridge, Southport, CT 06890	Unoccupied commercial building	0.13
27	136 James Street	N/A	1050-1	Fraser II LLC	17 Twin Circle Drive, Westport, CT 06880	Unoccupied industrial building	0.56
28	140 James Street	N/A	1050-1A	Fraser II LLC	17 Twin Circle Drive, Westport, CT 06880	Vacant area	0.21
29	129 James Street	N/A	1051-24	Mejia Juan	43 Sherman Street, Stamford, CT 06902	Vacant area used as parking lot	0.13
30	135 James Street	#139	1051-25	Mejia Juan	43 Sherman Street, Stamford, CT 06902	Vacant area used as parking lot	0.16

# Vacant and Underutilized Property



## E. Zoning

The Hollow zoning is largely R-C, residential/multi-family while a section is zoned Downtown Transit-oriented Development Village. An analysis of whether zoning matches any particular area or site was not performed as a part of this study.



### Hollow Zones

- R-A, Residential - A Single Family Zone
- R-B, Residential - B Two Family Zone
- R-C, Residential - C Multi-Family Zone
- MU-LI, Mixed Use - Light Industrial Zone
- OR, Office/Retail Zone
- OR-G, Office/Retail General
- DVD-TOD, Downtown Transit-Oriented Development Village
- DVD-CIVIC, Downtown Civic Village District
- DVD-BLVD, Downtown Boulevard Village District

### **3. Summary of Development Issues, Constraints, and Opportunities (contains info from the community workshop and walking tour)**

#### **A. Public Safety**

Public safety is a common concern for residents, and seems to be centered around a small number of locations and groups of people.

#### **B. Walkability, Sidewalks and Traffic**

Many sidewalks are in states of disrepair, and there is a lack of street trees and landscaping throughout the Hollow. Damage to curbs often comes from shortages in parking, causing drivers to park on sidewalks.

#### **C. Sanitation**

There is a lack of trash cans throughout the neighborhood and no service to pick up litter. Litter is a consistent problem, often concentrated around businesses and restaurants, and blowing into vacant lots.

#### **D. Blight and Vacancies**

There are many vacant lots and buildings that present opportunities for redevelopment.

#### **E. Parking**

There are parking shortages in many areas of the Hollow, especially in more commercial sections.

#### **F. Lack of Community Identity**

Residents feel that there is a lack of community identity in the Hollow. This includes both physical geographic identity and community identity among residents. The large population of renters contributes to this, as these residents do not feel as connected to the neighborhood as home owners.

#### **G. Lack of Community Centers and Outdoor Space**

There are few community centers and spaces for outdoor recreation. Lafayette/Nanny Goat Park is the only public park, and is often crowded or unsuitable for activities. There is a community center just outside the Hollow, but the interstate presents a barrier to accessing it.

#### **H. Lack of Community Partners**

The NRZ does not have many community partners that are also invested in improving the neighborhood.

### **4. Hollow NRZ Plan**

#### **A. Vision Statement**

The Hollow NRZ seeks to make their community a safer and more attractive place to live and raise a family.

## **B. Plan Overview – Goals & Objectives**

### **GOAL: Cleaner streets and neighborhood lots**

#### Objectives:

- Organize student and community clean up efforts
- Publicize the efforts and activities to cultural and community groups
- Request help from municipal services to enforce ordinances on trash
- Request police to take notice of littering and uncontained trash

### **GOAL: Improve parking within the community**

#### Objectives:

- Ask the City to confirm the conducted survey findings as part of the NRZ
- Use the parking study report to start discussion with residents and the city
- Petition the city to implement parking controls in The Hollow

### **GOAL: Improve streetscapes and gateways throughout The Hollow to build neighborhood identity**

#### Objectives:

- Establish signs and landscaping to indicate the entrances to The Hollow
- Establish “green teams” to care for public landscaping along various streets
- Seek funding from city and local businesses for streetscape improvements
- Reach out to community, religious, and city-wide non-profits for support

### **GOAL: Build Partnerships throughout the Neighborhood**

#### Objectives:

- Work more closely with police to increase safety and reduce crime
- Create partnerships with religious leaders and communities
- Create partnerships with non-profits and private businesses
- Create a communications plan to promote and educate the community

### **GOAL: Address chronically vacant or blighted properties**

#### Objectives:

- Collaborate with City in outreach to property owners, discussing the need for property (re)development.
- Connect property owners with competent developers.
- Pursue acquisition of parcels where the property owners does not have the desire or means to (re)develop. Eminent domain powers of the City shall be strategically used.

### **GOAL: Advocate with city for funding assistance to improve The Hollow**

#### Objectives:

- Seek housing rehabilitation assistance for existing housing stock
- Seek funding to assist in home ownership
- Seek funding assistance for facade improvements in commercial areas

## Appendices

1. Community Survey
2. Parking Reference Documents

## HOLLOW CONSTITUENT SURVEY CHECKLIST

### Demographic Information

- 0-10
- 11- 17
- 18 – 27
- 28 – 50
- 51 – 65
- 66 – older
  
- Male
- Female
  
- Black (African American)
- Hispanic, Latino, Caribbean
- White (Caucasian)
- American Indian, Alaska Native
- Asian
- Native Hawaiian-Other Pacific Islander
- Other

Country of origin:

### YEARS LIVED OR WORKED IN THE HOLLOW

- 1 - 3
- 4 - 7
- 8 - 11
- 12 - 20
- 21+

### BUSINESS or OCCUPATION

NEIGHBORHOOD CONCERNS 1 = Excellent and 10 = Very Bad

How Do You feel about the following:

- Public Safety
- Walkability/sidewalks
- Traffic/congestion
- Vacant Property/ Empty Buildings
- Blight/Graffiti
- Parking
- Litter/Sanitation/Trash Collection
- Trash Can/Places to put waste
- Community or Neighborhood Identity
- Is there a Community Center
- How is outdoor space / recreation

# HOLLOW CONSTITUENT SURVEY CHECKLIST

What are YOUR top 3 CONCERNS with regard to the HOLLOW?

1

2

3

What are YOUR top THREE NEEDS with regard to your business, organization or family in order to thrive in the Hollow?

1

2

3

Would you like to be added to the HOLLOW NRZ email list? \_\_\_\_\_ No Thank You

\_\_\_\_\_ YES, please.

Email Address:

Would you like to become involved with the HOLLOW NRZ planning? \_\_\_ No Thank You

\_\_\_\_\_ YES, please.

NAME \_\_\_\_\_

ADDRESS \_\_\_\_\_

PHONE NUMBER \_\_\_\_\_

Is this a cell phone and if yes, say we send Text Messages \_\_\_\_\_ No Thank You

\_\_\_\_\_ YES, please.

## NEXT PUBLIC MEETING

City Hall (Old high school, not downtown)

145 Lyon Terrace

Wheeler Room A & B (Next to the Common council Chambers)

**November 28<sup>th</sup> 6:30 PM**

# Appendix 2: Parking Reference Documents

Data Book for Civil Engineers, E.E. Seelye, very little has changed in over 75 years with regard to the amount of land needed for automobile parking.

12-77

## PARKING — FIELDS

Remarks	See Fig. B					See Fig. B		
	Col. 1	Col. 2	Col. 3	Col. 4	Col. 5	Col. 6	Col. 7	Col. 8
Notes: Dimensions vary with vehicle size, parking function, and degree of activity. Stall and aisle widths generally have an inverse relationship. Dimensions listed are minimum proven effective for U.S. 1958 autos. (When space is no problem, minimum stall width should be 9'-0").	Angle of Parking (degrees)	Stall Width	Width of Stall perpendicular to aisle	Depth of Stall perpendicular to aisle	Width of Aisle	Parking quantity Walls or bumper stops	Parking with interlocking stalls	Parking head-in to curb
General All Purpose Standard, 1958	90	8'-9"	8'-9"	18'-0"	26'-0"	62'-0"	—	58'-2"
All-day and low turnover lots (min: COL. 5-23'; COL. 6-59')	90	8'-6"	8'-6"	18'-0"	26'-0"	62'-0"	—	58'-2"
Large all-day parking lots. (Approx. 170 cars/acre)	90	8'-6"	8'-6"	18'-0"	24'-0"	60'-0"	—	—
Inadequate room for opening car doors. Rarely used.	90	8'-0"	8'-0"	18'-0"	32'-0"	68'-0"	—	64'-2"
Typical pay parking field (high turnover)	90	8'-6"	8'-6"	18'-0"	27'-6"	63'-6"	—	59'-8"
Desired dimensions (for 8'-6" stalls)	90	8'-6"	8'-6"	18'-0"	29'-0"	65'-0"	—	61'-2"
Desired dimensions (for 9'-0" stalls)	90	9'-0"	9'-0"	18'-0"	27'-0"	63'-0"	—	59'-2"
Active shopping centers without separate pedestrian walkways and high turnover lots where ample land is available.	90	9'-0"	9'-0"	18'-6"	30'-0"	67'-0"	—	63'-2"
Generally recommended minimum (8'-6" stalls)	60	8'-6"	9'-10"	19'-10"	18'-0"	57'-8"	53'-5"	52'-7"
Generally recommended minimum (9'-0" stalls)	60	9'-0"	10'-5"	20'-1"	17'-0"	57'-2"	52'-8"	51'-10"
General all-purpose minimum and in clear span, one-way aisle, self-park garages.	45	8'-6"	12'-0"	18'-9"	12'-6"	50'-0"	44'-0"	44'-10"
Minimum, for special purposes only	45	8'-0"	11'-4"	18'-5"	12'-0"	48'-10"	43'-2"	44'-0"
Minimum for short aisles	45	9'-0"	12'-9"	19'-1"	11'-4"	49'-6"	43'-2"	44'-0"
Generally recommended minimum (8'-6" stalls)	30	8'-6"	17'-0"	16'-5"	10'-0"	42'-10"	35'-9"	40'-11"
Generally recommended minimum (9'-0" stalls)	30	9'-0"	18'-0"	16'-10"	9'-0"	42'-8"	35'-6"	40'-9"
SELECTED STALL AND AISLE DIMENSIONS FOR BACK-IN PARKING. USED PRIMARILY FOR ATTENDANT PARKING.								Parking Back-in to Curb
Attendant parking only	90	8'-0"	8'-0"	18'-0"	22'-0"	58'-0"	—	51'-2"
Minimum for customer self-parking. Aisle and unit depth preferably increased by 1' or 1'-6"	90	8'-6"	8'-6"	18'-0"	21'-0"	57'-0"	—	50'-2"
Used where maximum number of two-way aisles is desired.	90	9'-0"	9'-0"	18'-0"	20'-0"	56'-0"	—	49'-2"

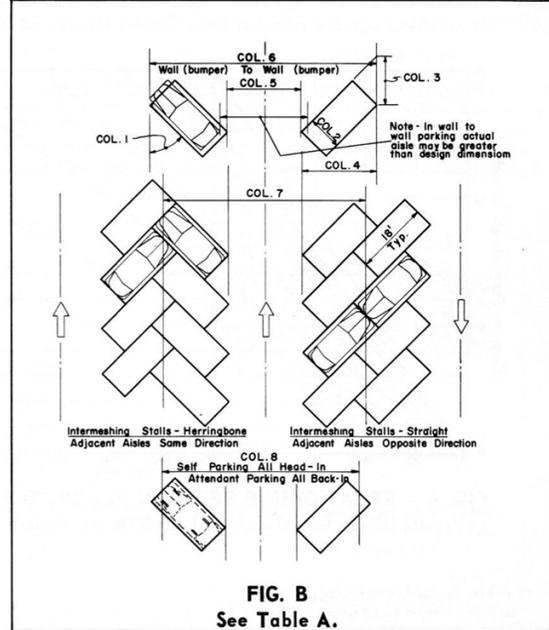


FIG. B  
See Table A.

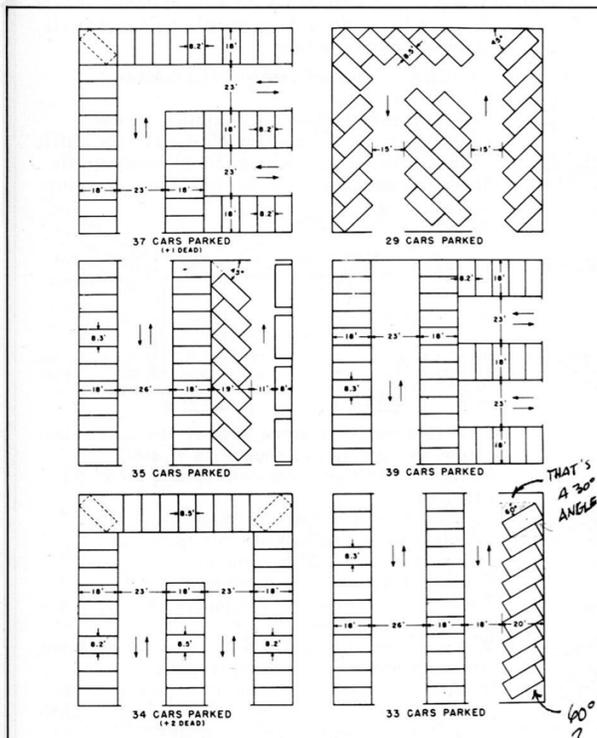
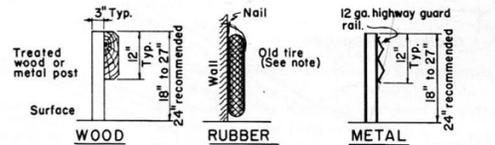


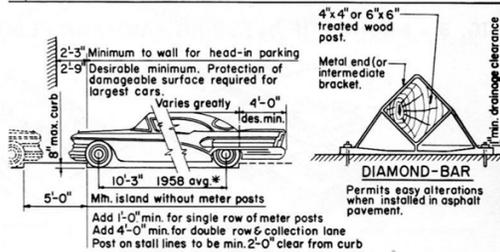
FIG. E - 6 EXAMPLES OF 100' x 100' SELF-PARKING FIELDS

From *Parking by The* Eno Foundation For Highway Traffic Control, 1957.



NOTE - Manufactured rubber fenders, in many shapes, are available for wall and loading docks.

(1) - BUMPER STOPS (Most efficient for space layout)



CURB - Concrete, wood or metal

Head-in parking recommended

Back-in parking not recommended

\* See p. 12-79 for design vehicle dimensions.

(2) - WHEEL STOPS (Generally preferred by motorists)

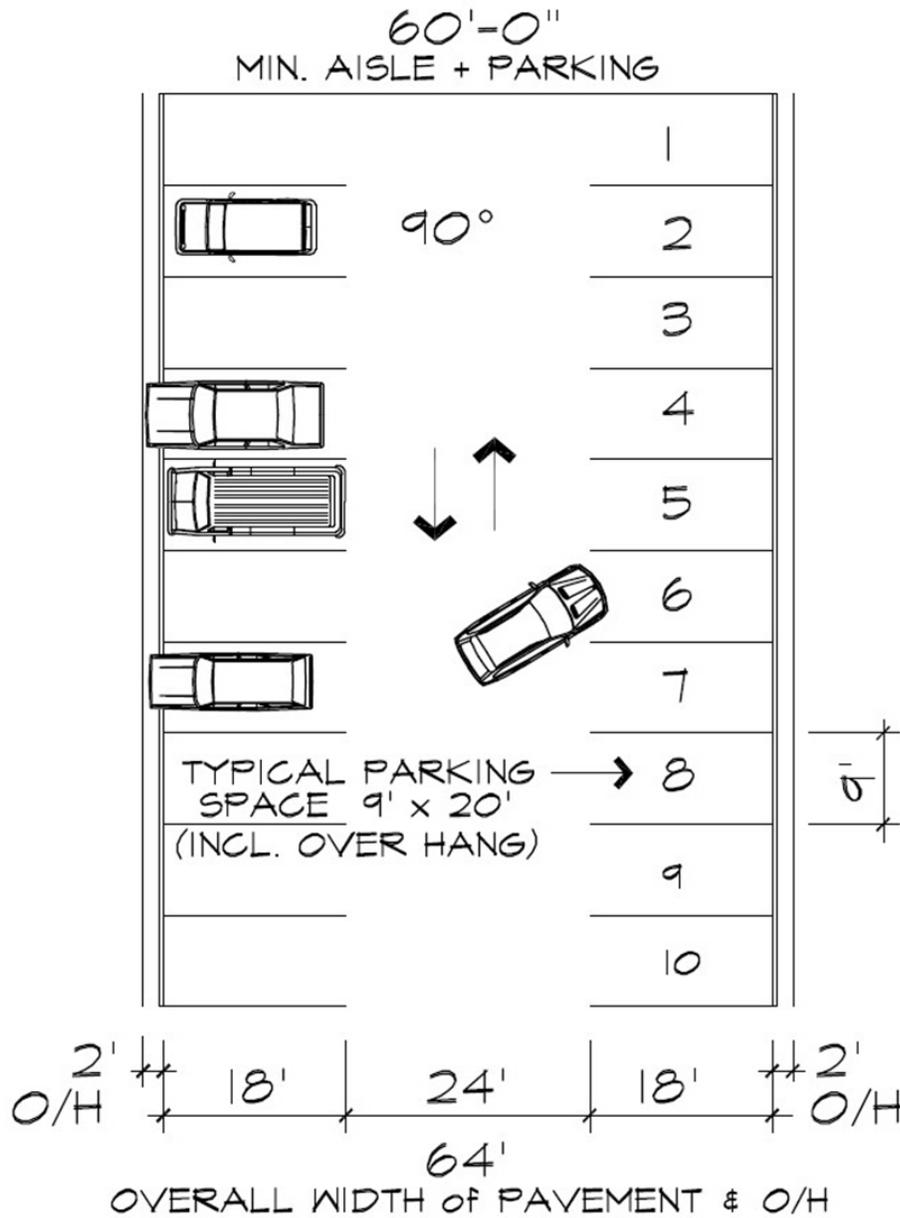
FIG. C - CAR STOPS - PARKING FIELDS

TABLE F - GRADES: PARKING FIELD	TABLE G - TYPICAL RANGE OF 90° PARKING FIELD, GROSS AREA PER CAR	
Minimum . . . . . As required for drainage	Based on commercial lots with greater than 50-car capacity.	
Desirable maximum . . . . . 4 %		
Maximum transverse to parking stall . . . . . 6.5%		
TABLE H - AREA LIGHTING: PARKING FIELD	Parking Type	Gross, sq.-ft./car
Desirable minimum: ½ to 2 ft.-candles, increase up to 5 ft.-candles for non-attended meter fields.	Attendant, back-in	200
	Attendant, head-in	225
	Customer, minimum	250
	Customer, roomy	300

(Parking Cont.)

**City of Bridgeport Zoning Regulations:** The City of Bridgeport recently relaxed some of its standards with regard to overhang and the "Standard Parking Space" being 9' x 20'. In February 2017 they accepted a standard for commercial area parking that was 9' x 18'. This is recognition that more and more automobiles are getting smaller. The accompanying drawings below use the typical 9'x20' space.

**TYPICAL 60' WIDE PARKING LOT**



(Parking, cont.)

City of Bridgeport Zoning Regulations: Angled parking spaces

